

INDEX OF SHEETS

- 1 SKETCH MAP AND TITLE PAGE
- 2 TYPICAL SECTION - SUMMARY OF QUANTITIES (CONST. DIV. NO. 2)
- 3 SUMMARY OF QUANTITIES (CONST. DIV. NO. 1)
- 4 & 5 LIST OF STRUCTURES
- 6 FENCING, GUARD POSTS, & SURFACING PLAN
- 7-9 DETAILS OF BRIDGE, STA. 1539+
- 10 " " " BRIDGE, STA. 1539+
- 11-14 " " " BRIDGE, STA. 978+
- 15 " " " DETOUR BRIDGE, STA. 978+
- 16 & 17 DETAILS OF BRIDGE STA. 673+
- 18 " " " DETOUR BRIDGE, STA. 673+
- 19-22 " " " BRIDGE, STA. 41+
- 23 " " " DETOUR BRIDGE, STA. 41+
- 24 STANDARD TIMBER GUARD POSTS
- 25 " " " WIRE FENCES (TREATED WOODEN POSTS)
- 26 " " " STRUCTURE NUMBER LETTERING
- 27 " " " YEAR NUMBER MARKING
- 28 TYPICAL SIDE APPROACH ROADS - ROADWAY CONSTRUCTION TRAFFIC SIGNS
- 29 STANDARD MARKER POST
- 30-33 ALIGNMENT, PLAN, AND PROFILE
- 34-52 CROSS SECTIONS

COLORADO

STATE HIGHWAY DEPARTMENT

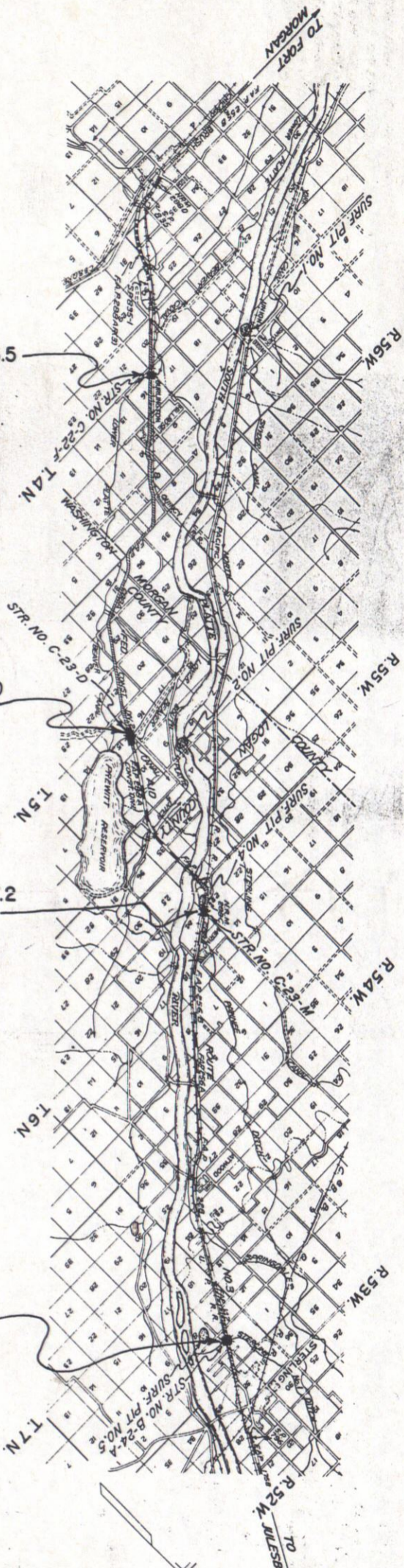
PLAN AND PROFILE OF PROPOSED  
 COMB. FEDERAL AID PROJECT NO. 288 A (2)  
 288 B (2), & 142(3)  
 STATE HIGHWAY NO. 2  
 MORGAN, WASHINGTON, & LOGAN COUNTIES

SCALES ON ORIGINAL TRACINGS

ON PLAN, 1 IN. = 100 FT.  
 ON PROFILE 1 IN. = 10 FT. HORIZONTAL  
 1 IN. = 10 FT. VERTICAL

GRADE LINE ON PROFILE IS SHOWN AS GRADE OF FINISHED ROAD

	UNIT 1	UNIT 2
GROSS LENGTH OF PROJECT	626.9 FT. = 0.119 MI.	1200.0 FT. = 0.227 MI.
NET LENGTH OF PROJECT	626.9 FT. = 0.119 MI.	1200.0 FT. = 0.227 MI.
GROSS LENGTH OF PROJECT	657.2 FT. = 0.124 MI.	-
NET LENGTH OF PROJECT	632.0 FT. = 0.119 MI.	-
PROJECT LENGTHS	55107.1 FT. = 10.436 MI.	18269 FT. = 0.346 MI.
	657.2 FT. = 0.124 MI.	632.0 FT. = 0.119 MI.

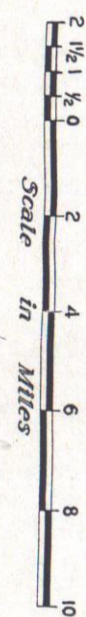


STA. 1536+46.6 TO 1542+73.5  
 FAP 288 A (2)-UNIT 1

STA. 974+00 TO 986+00  
 FAP 288 A (2)-UNIT 2

STA. 670+40 TO 676+97.2  
 FAP 288 B (2)-UNIT 1

STA. 38+85 TO 45+17  
 FAP 142 (3)-UNIT 1



- CONVENTIONAL SIGNS
- CENTER LINE OF SWAYEY
  - RIGHT OF WAY LINES
  - TOWNSHIP OR RANGE LINES
  - SECTION LINES
  - ONE-QUARTER SECTION LINES
  - BARBED WIRE FENCE
  - GUARD FENCE
  - TIMBER GUARD POSTS
  - TELEPHONE LINES
  - POWER LINES

142,0301

FED. ROAD DIST. NO.	STATE	COMB. F. A. PROJ. NO.	SHEET NO.	TOTAL SHEETS
3	COLO.	288 A(2), 288 B(2) & 142(3)	1	

NOTE:  
 It is recommended that bidders on this Project go over the plan details with one of the following field representatives of this department:  
 Roy J. Randall, Division Engineer, Greeley, Colo.  
 W.A. Lewis, Resident Engineer, Brush, Colo.

APPROVED FOR APPROVAL

APPROVED

APPROVED

RECOMMENDED FOR APPROVAL

RECOMMENDED FOR APPROVAL

RECOMMENDED FOR APPROVAL

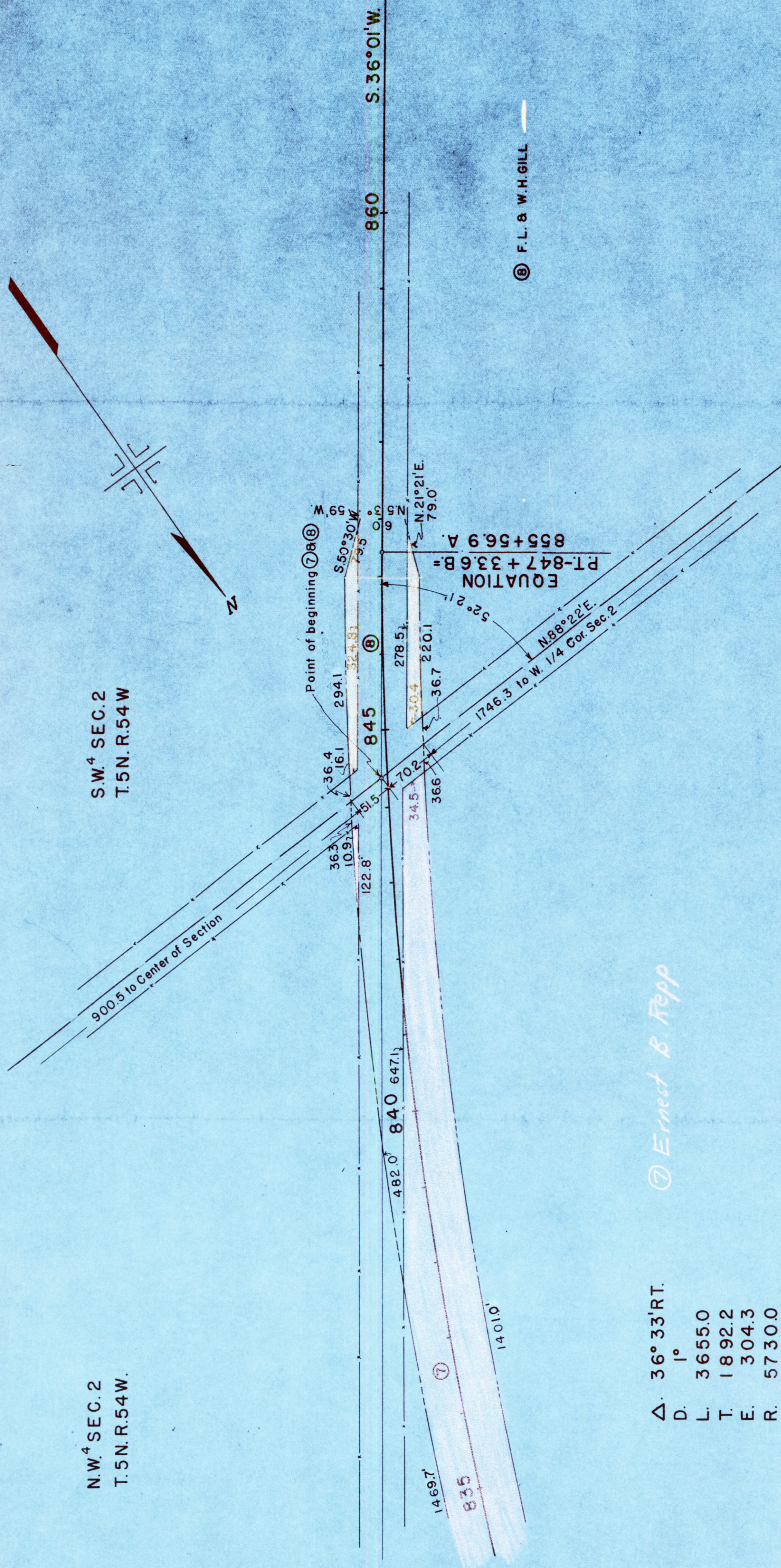
APPROVED

STATE	F. APPROJ. NO.	SHEET NO.	TOTAL SHEETS
STATE	288 A (2)	2	3

STATE	COLO.
3	

N.W.<sup>4</sup> SEC. 2  
T.5N. R.54W.

S.W.<sup>4</sup> SEC. 2  
T.5N. R.54W.



Ⓞ F.L. & W.H. GILL

⑦ Ernest B. Fepp

Δ. 36° 33' RT.  
D. 1°  
L. 3655.0  
T. 1892.2  
E. 304.3  
R. 5730.0

PLAN	DATE	BY
SURVEYED		
PLOTTED		
GRADES CHECKED		
NOTE BOOK		
RT. OF WAY CHECKED		
NO.		

PROF.	DATE	BY
SURVEYED		
PLOTTED		
GRADES CHECKED		
NOTE BOOK		
STRUCTURE NOTED AS CHECKED		
NO.		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	GRADES CHECKED		
	B. M. NOTED		
	STRUCTURE NOTATED		

PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED	W.R.D.	
NO.	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		

NW<sup>4</sup> SEC. 15  
T. 5N. R. 54W.

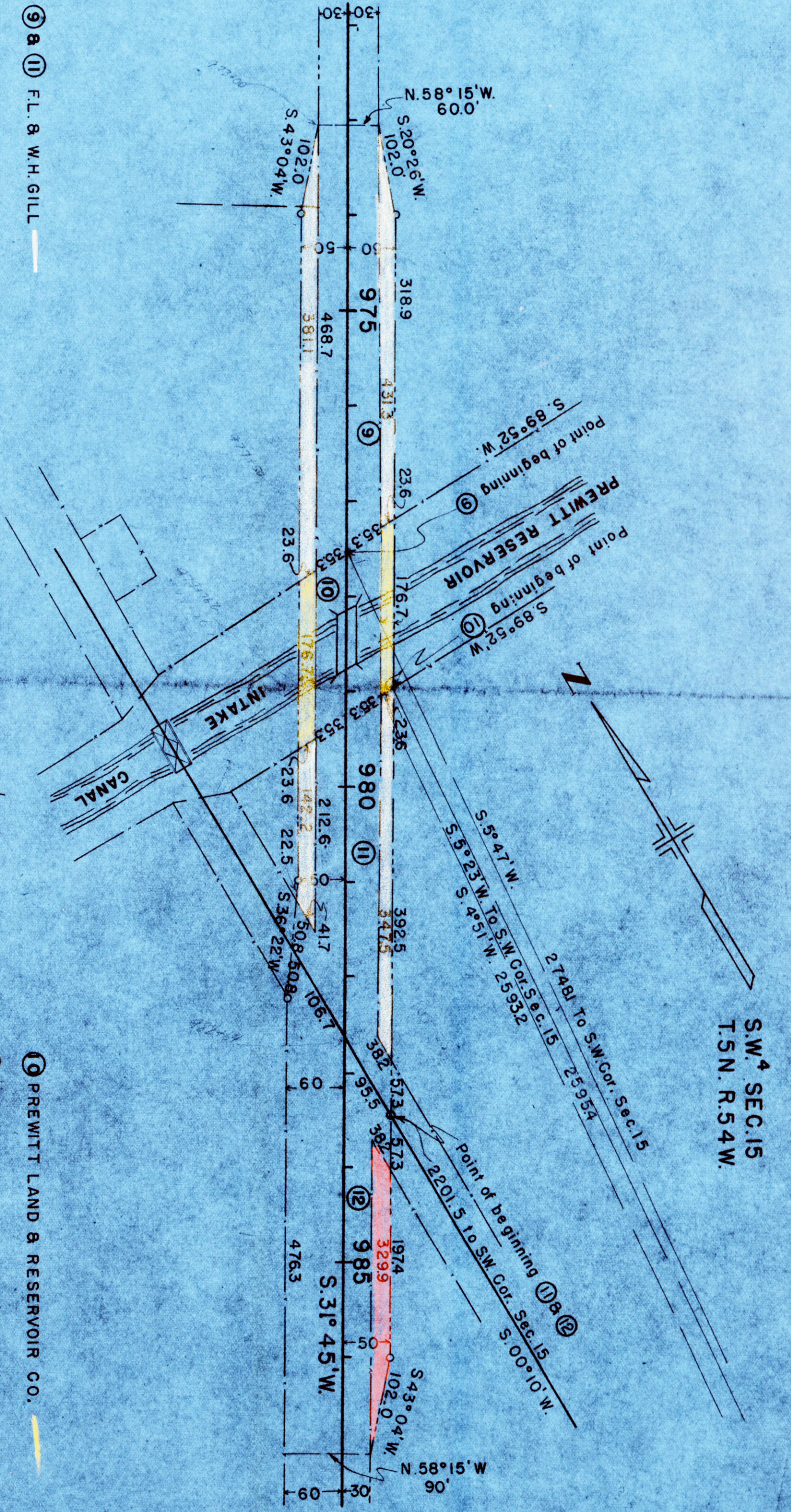
NE<sup>4</sup> SEC. 16  
T. 5N. R. 54W.

⑨ & ⑩ FL. & W.H. GILL

⑪ PREWITT LAND & RESERVOIR CO.  
⑫ STATE OF COLORADO

SE<sup>4</sup> SEC. 16  
T. 5N. R. 54W.

S.W.<sup>4</sup> SEC. 15  
T. 5N. R. 54W.

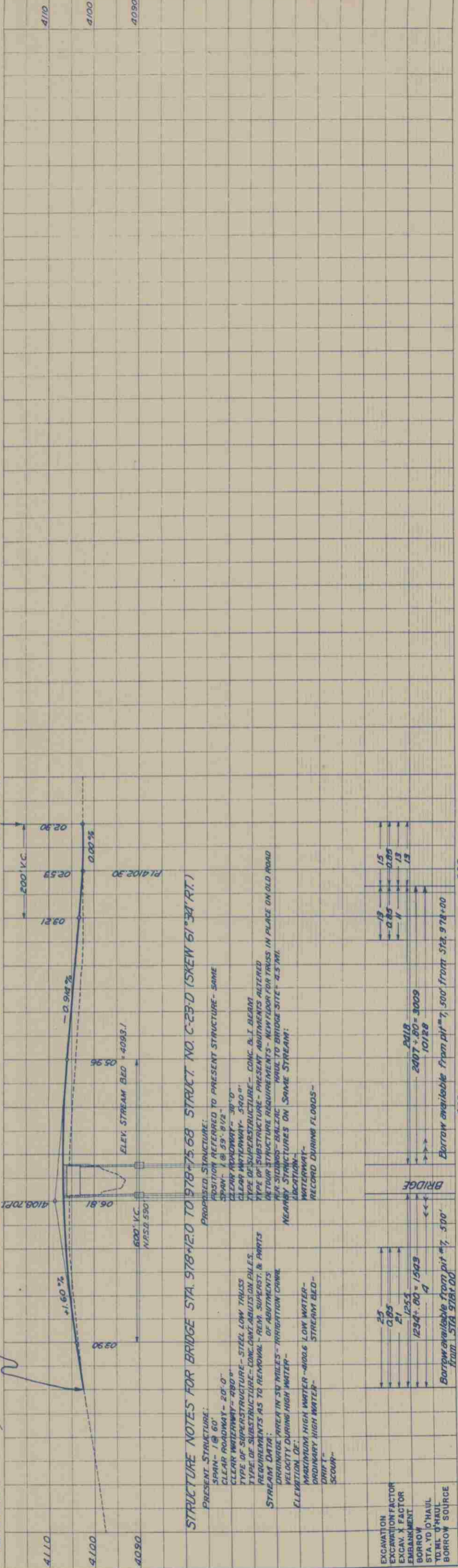
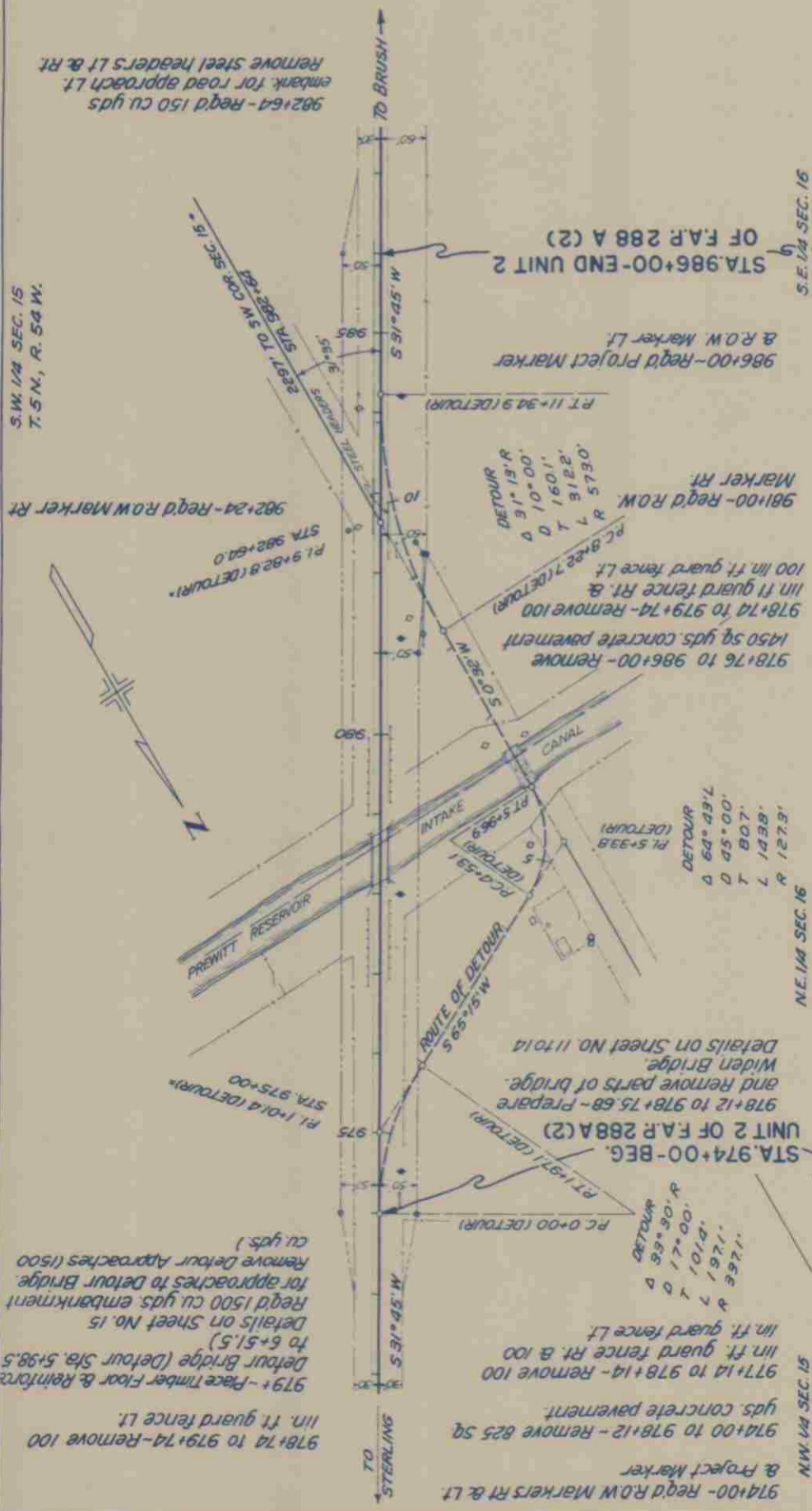


FED. ROAD DIST. NO.	STATE	PA. PROJ. NO.	SHEET NO.	TOTAL SHEETS
3	COLO.	288 A (2)	3	3

PLATE 1 - PLAN - PROFILE of A. M. & S. STAVANER  
KURTZ & SUTHERLAND CO., NEW YORK



**NOTE:**  
 Grades as shown are subject to modification during construction to meet existing pavement grades.  
 All poles encroaching on the construction are to be moved by the owners.



673+20 to 673+40 - Req'd approach slab. Details on Sheet No. 16. Remove 40 sq yds concrete pavement.

673+31 to 673+51 - Remove 20 lin. ft. guard fence Lt.

S.W. 1/4 SEC. 24  
T.6 N. R.54 W.

S.E. 1/4 SEC. 23

STA. 670+40 BEG. UNIT I OF F.A.P. 288 B (2)

673+21 to 673+51 - Remove 30 lin. ft. guard fence Rt.

673+40 to 673+97.2 - Req'd Bridge & 400 cu. yds excavation for channel improvement. Details on Sheets No. 16 & 17. Remove Bridge.

N.E. 1/4 SEC. 23.

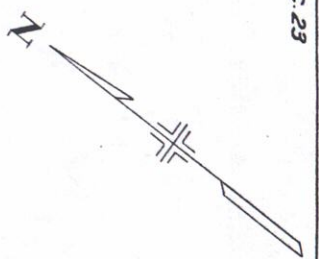
674+11 to 674+40 - Remove 30 lin. ft. guard fence Rt.

STA. 676+97.2 END UNIT I OF F.A.P. 288 B (2)

673+97.2 to 674+17.2 - Req'd approach slab. Details on Sheet No. 16. Remove 17 sq. yds concrete pavement.

674+11 to 674+40 - Remove 30 lin. ft. guard fence Lt.

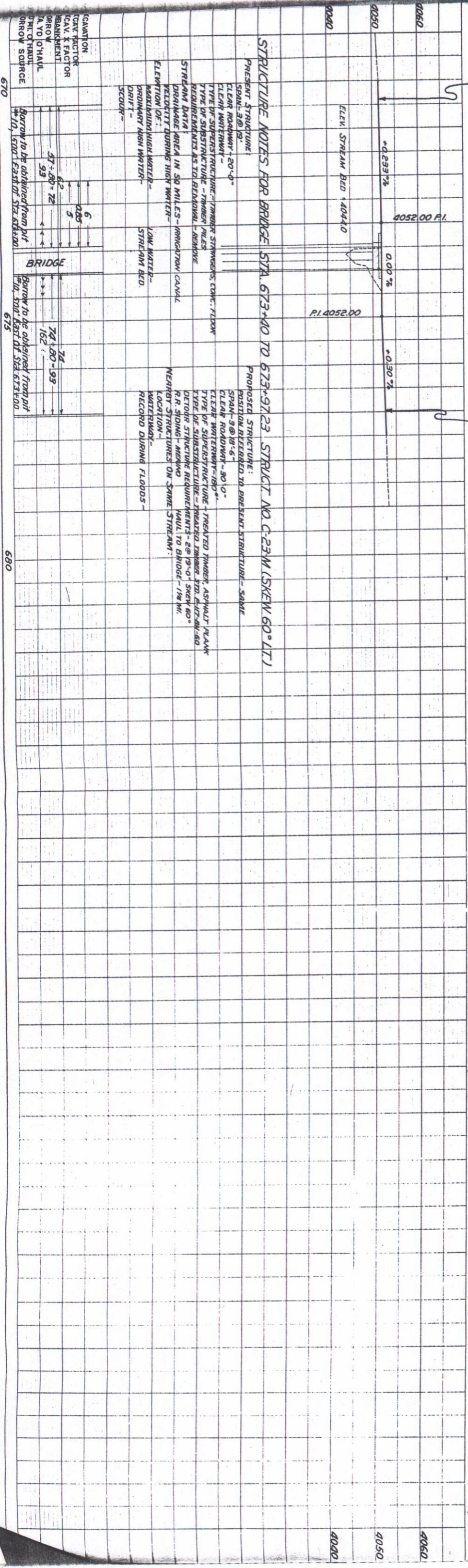
672+ Req'd Detour Bridge Rt. Details on sheet No. 18. Req'd 200 cu. yds. embank. for approaches to Detour Bridge. Remove Detour Bridge & approaches (200 cu. yds. excav.).



NOTE:  
Grades as shown are subject to modification during construction to meet existing pavement grades.  
All poles encroaching on the construction are to be moved by the owners.

FED. ROAD DIST. NO.	STATE COLO.	F.A. PROJ. NO.	SHEET NO.	TOTAL SHEETS
3		208B(2)	32	

Rev. 6-21-41 Sheet No. R.K.C.



STRUCTURE NOTES FOR BRIDGE STA. 673+40 TO 673+97.23 STRUCT NO. C-23M (SKEW 60° LT)

PRESENT STRUCTURE:  
SPAN - 3 @ 19'-6"  
CLEAR ROADWAY - 20'-0"  
TYPE OF SUPERSTRUCTURE - TIMBER STRINGERS, CONC. FLOOR  
REQUIREMENTS AS TO BEADINGS - REMOVE  
DRAINAGE AREA IN 50 MILES - IRRIGATION CANAL  
VELOCITY DURING HIGH WATER -  
ELEVATION OF -  
MAXIMUM HIGH WATER -  
ORDINARY HIGH WATER -  
DRIFF -  
SCOUR -  
LOW WATER -  
STREAM BED -

PROPOSED STRUCTURE:  
POSITION REFERRED TO PRESENT STRUCTURE - SAME  
SPAN - 3 @ 19'-6"  
CLEAR ROADWAY - 20'-0"  
TYPE OF SUPERSTRUCTURE - TREATED TIMBER ASPHALT PLANK  
REQUIREMENTS AS TO BEADINGS - 2 @ 19'-0" SKEW 60°  
DRAINAGE AREA IN 50 MILES - IRRIGATION CANAL  
VELOCITY DURING HIGH WATER -  
ELEVATION OF -  
MAXIMUM HIGH WATER -  
ORDINARY HIGH WATER -  
DRIFF -  
SCOUR -  
LOW WATER -  
STREAM BED -  
RECORD DURING FLOODS -

CAUTION  
GRAV. FACTOR  
ICAV. FACTOR  
BRANKMENT  
ORROW  
M. YD. OF HAIL  
ORROW SOURCE

Bottom to be obtained from pit #10, 5100 East of Sta. 673+500

Bottom to be obtained from pit #10, 5100 East of Sta. 673+500



