



COLORADO
Department of
Transportation

DOCUMENT SEPARATOR SHEET

REGION 5 – JUNE 2017 CONVERSION

To be placed at the beginning of each separator sheet.



r500001730

Description:

ROW Plans 11X17

Route # and Mile Points:

SH 145

Originating Office:

ROW/Survey

File Name:

1-E-1_ROW(.PDF)

Box Location:

27 of 38

STATE OF COLORADO

DEPARTMENT OF TRANSPORTATION

4201 East Arkansas Avenue, 4th Floor
Denver, Colorado 80222-3400
FAX 767-9868



COLORADO DEPARTMENT OF TRANSPORTATION FACSIMILE TRANSMITTAL

DATE <i>04/28/06</i>	TIME	COPIES SENT (INCLUDE THIS FORM) <i>Two</i>	LONG DISTANCE ACCESS CODE (IF NECESSARY)
TO: <i>Dan Fazzan</i>		PHONE NO. (NOT FAX NUMBER)	
		FAX NO.	
FROM: <i>Doug Bennett</i>		PHONE NO.	
PRIORITY		<input type="checkbox"/> ADDRESSEE WILL PICKUP	<input type="checkbox"/> NOTIFY ADDRESSEE
		<input type="checkbox"/> DELIVER WITHIN 4 HOURS	
COMMENTS			

Per our phone conversation.

*THIS LETTER FROM DOUG BENNETT
NOTES THAT THE COUNTY OF MONTEZUMA
HAS ADVISED THAT THE ROW FOR CONNECTION
OF COLO. PROJ. 1-E1 HAS BEEN ACQUIRED.*

*DF
4-28-06*

FILE

May 4, 1949

Mr. A. V. Williams,
District Engineer,
Federal Works Agency,
375 New Center State Building,
Denver, E. Colorado.

Dear Sir:

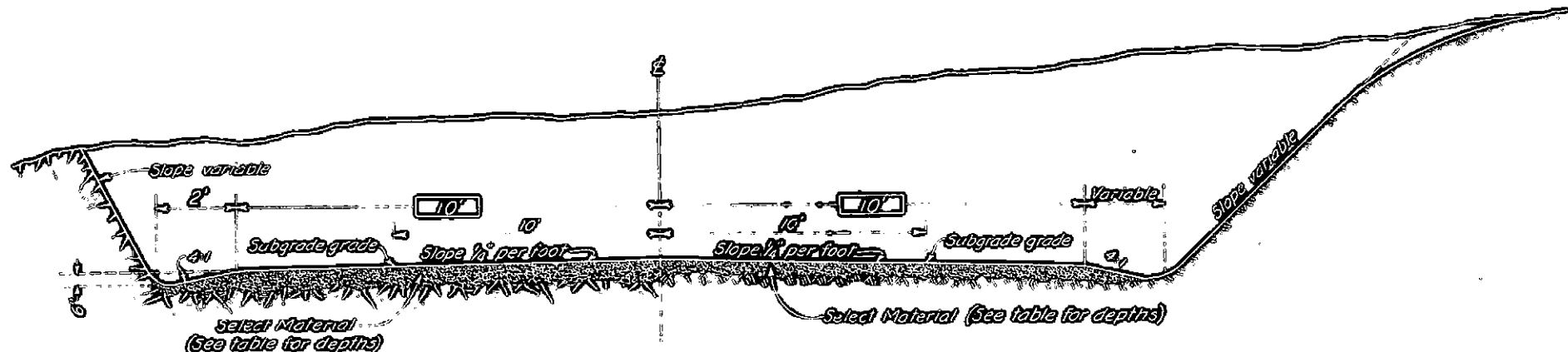
The County of Washington has advised that the right-of-way
necessary for the construction of Colorado Forest 1-21, Bellevue-
to Rice, has been acquired.

Yours very truly,

WILLIAM W. WILSON,
State Highway Engineer.


W. W. WILSON,
Highway Engineer.

cc: Mr. of Survey and Plans, Denver, Colo. ✓
Mr. J. E. Brady, Denver, Colo.
Mr. J. L. Denver, Colo.



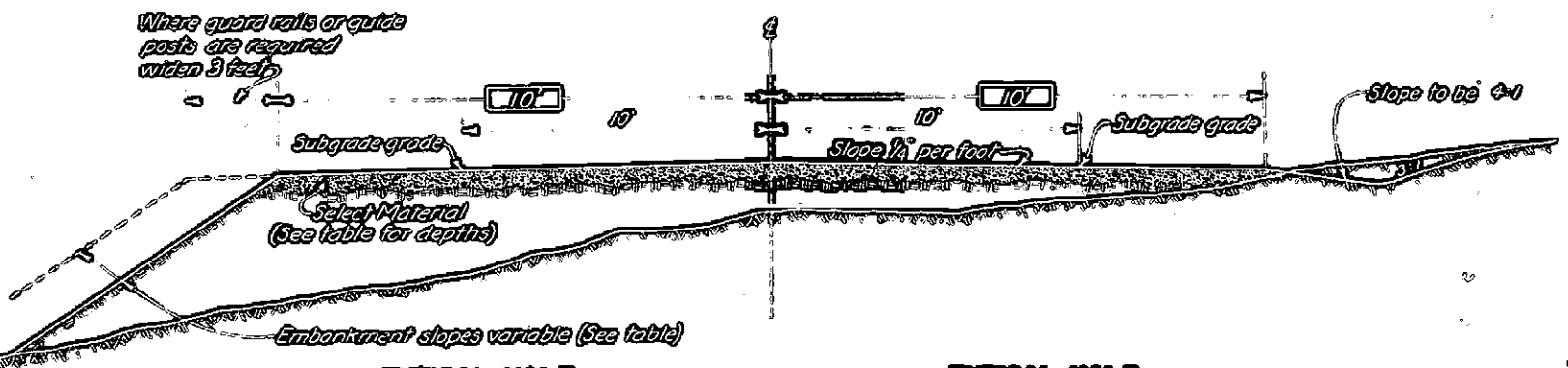
TYPICAL HALF CUT SECTION IN ROCK

TYPICAL HALF CUT SECTION IN COMMON

GENERAL NOTES:

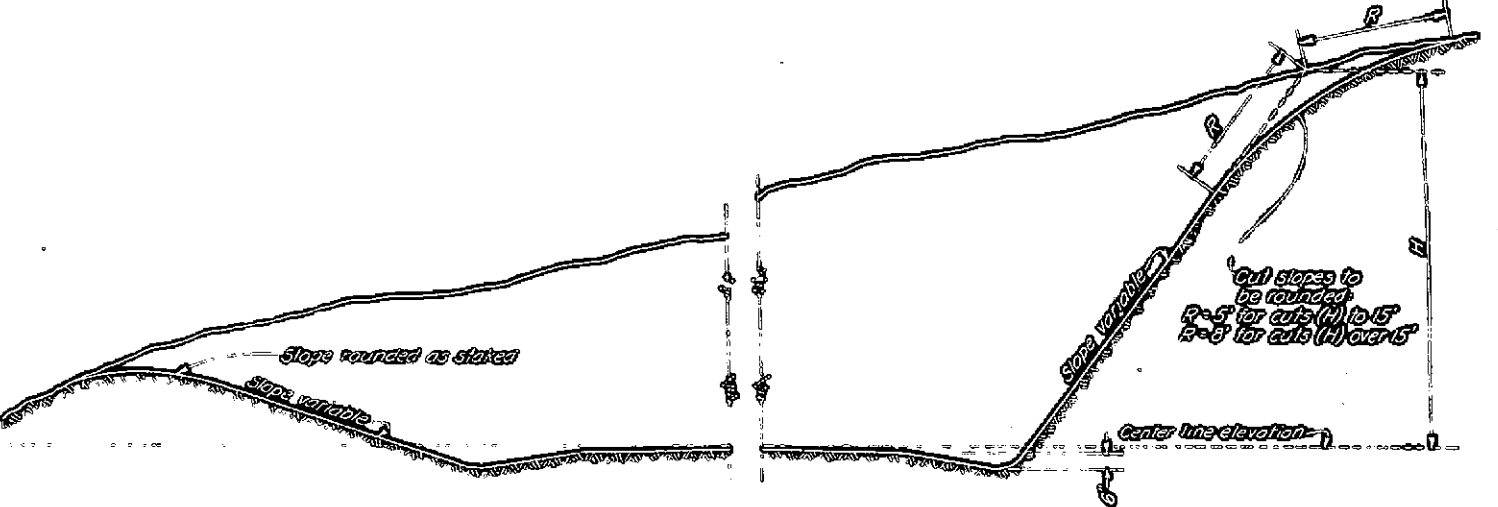
When borrow is specified in the contract and satisfactory material is found in the roadway excavation the right is reserved to increase the amount of excavation and reduce the unexcavated excavation for borrow. When additional material is needed for completing embankments, finishing the subgrade, or for selected cushion or topping it may be secured by uniformly widening the cut or sidehill cut or flattening the cut slopes where satisfactory material is available. The slopes of the ends of all cuts shall be flattened and tined to improve appearance. Narrow ditches shall be constructed on approximate one per cent grades following the ground contour and when possible shall be so constructed that the direction of flow will be away from the roadway.

Tops of soil shall be preserved and either placed in stockpiles or spread over cut and embankment slopes as directed and in accordance with the specifications. Roadway ditches at the ends of cuts shall be constructed so as to carry the flow away from the adjacent embankment slopes. Embankment slopes shall be uniformly warped between one rate of slope and another the transition shall cover a distance of not less than fifty feet. Subgrade grade to be on the surface of stabilized graded rock and located ten feet from center line on tangents and insides of curves. Public Roads Administration book of Transition Curves for Highways shall be used to determine superelevation and transition lengths (table I) and widening of curves (table II).

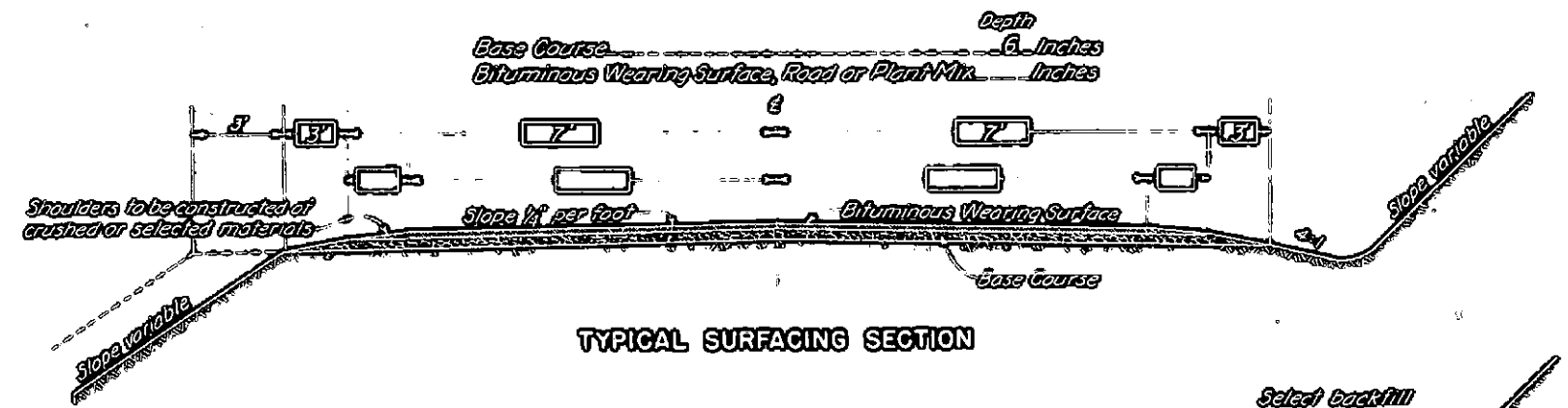


TYPICAL HALF EMBANKMENT SECTION

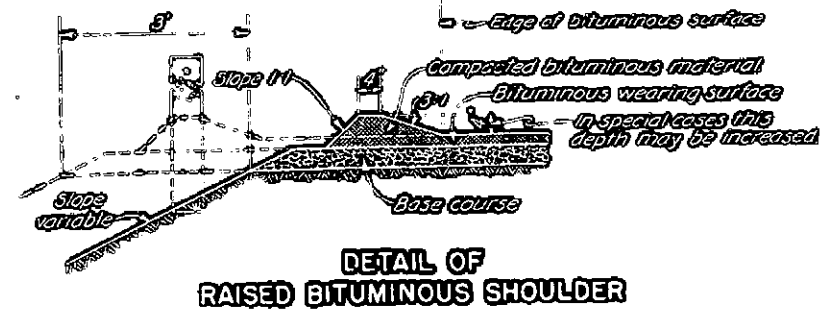
TYPICAL HALF SHALLOW EMBANKMENT SECTION



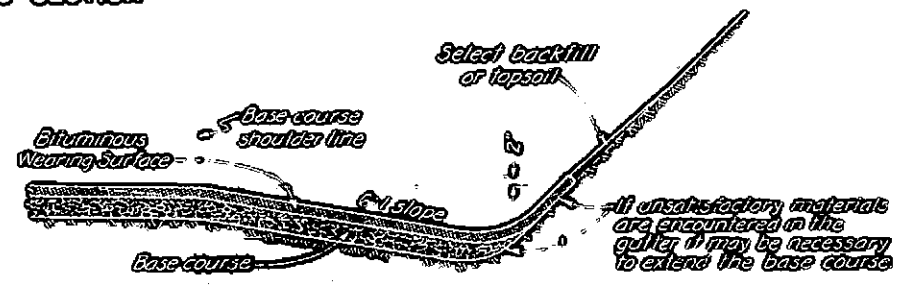
SLOPE TREATMENT IN EARTH CUTS



TYPICAL SURFACING SECTION



DETAIL OF RAISED BITUMINOUS SHOULDER



DETAIL OF BITUMINOUS GUTTER

SELECT MATERIAL

STATION TO STATION	DEPTH

SLOPES

HEIGHT	EMBANKMENT SLOPE	EARTH CUT SLOPE	ROCK CUT SLOPE
0' to 3'	6:1	3:1	3:0
3' to 6'	4:1	3:1	3:0
6' to 12'	2:1	2:1	0:1
12' to 30'	1:1	1:1	0:1
30' up	1:1	0:1	0:1

When field conditions indicate the need for slopes other than those indicated above they shall be constructed as staked by the Engineer. In cases where the height of cuts are in excess of 30 feet and in material which grades readily use slopes of 1 1/2 to 1 or better if practical.

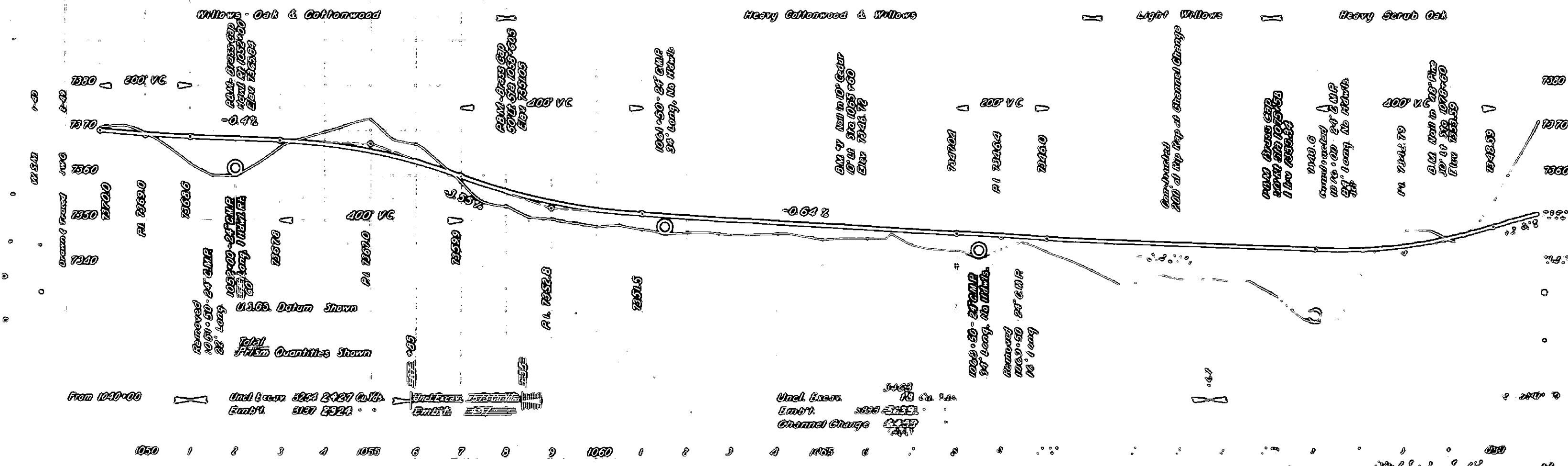
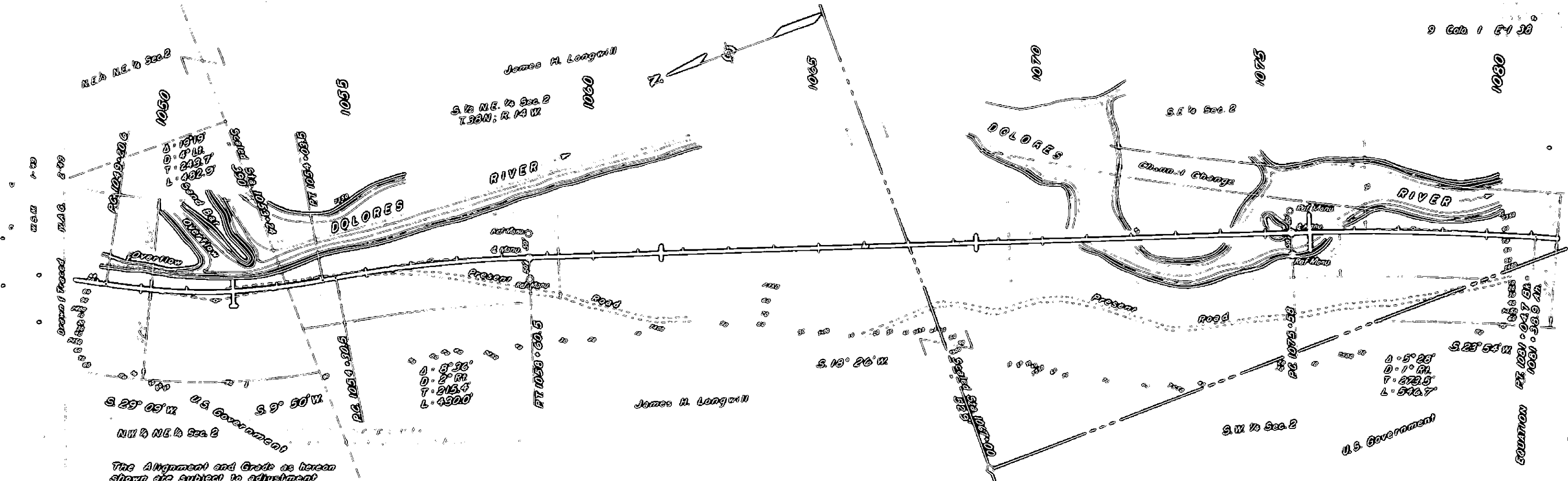
FEDERAL WORKS AGENCY
PUBLIC ROADS ADMINISTRATION
DIVISION NO. 9 DENVER, COLORADO

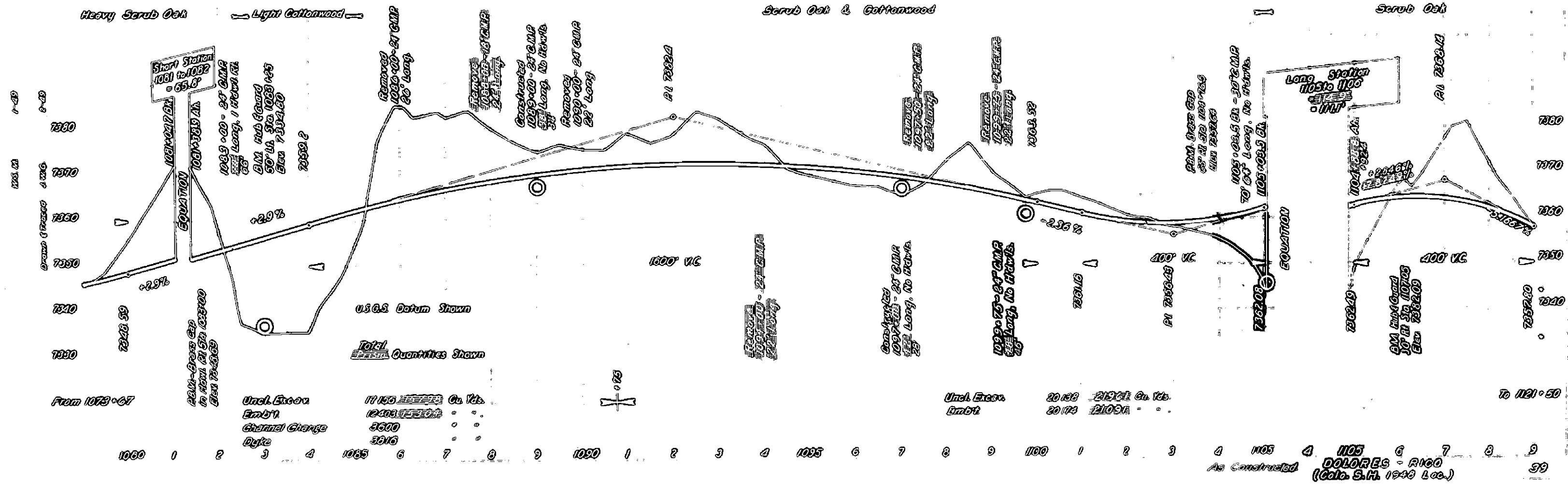
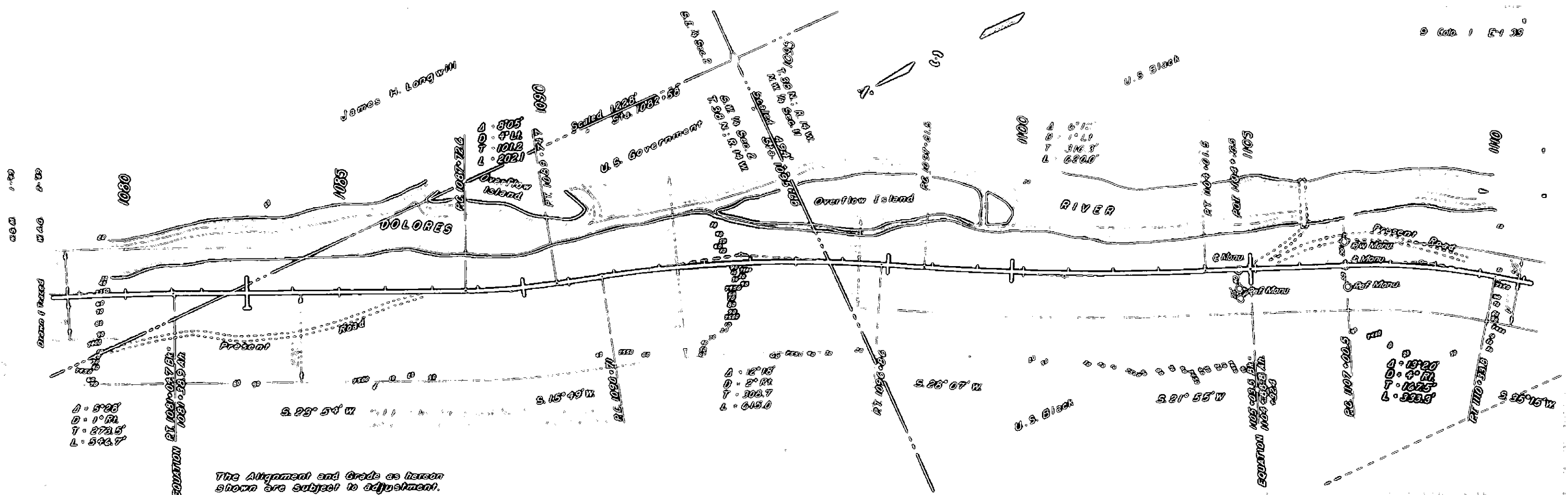
**TYPICAL CROSS SECTIONS
NATIONAL FOREST & PARK HIGHWAYS**

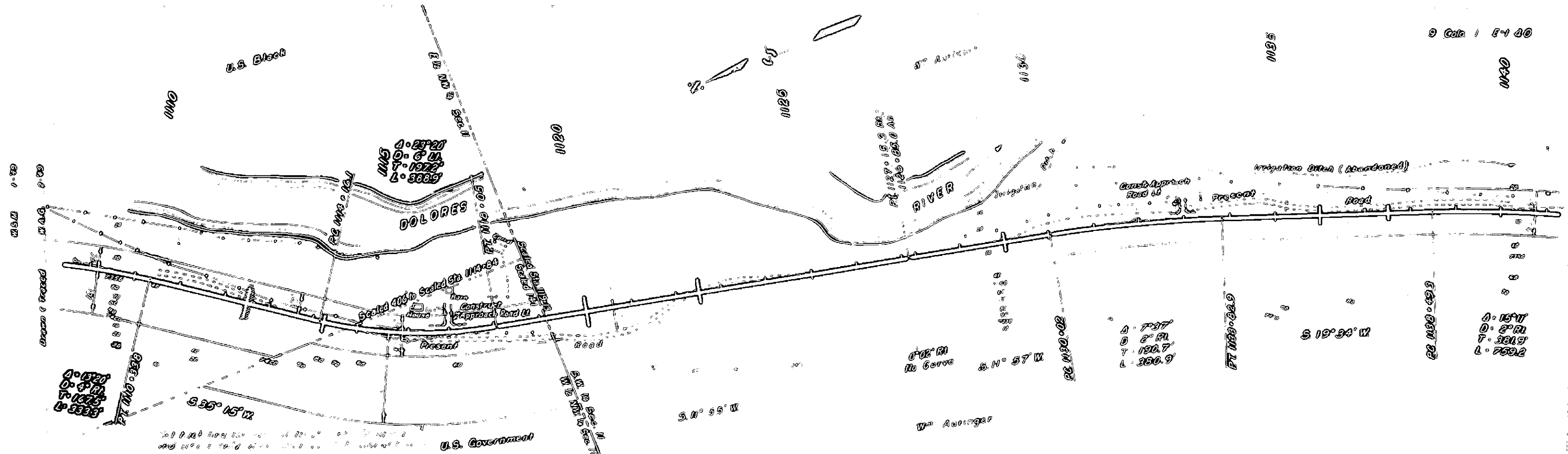
GRADED 20' Shoulder to Shoulder.
BASE COURSE 14'
BITUMINOUS WEARING SURFACE NOT SHOWN ON THIS COURSE

DIVISION No. 9 STANDARD ROADBED
PROJECT Galati - El Dolores - Rico
NATIONAL Forest & San Juan
COUNTY Montezuma
STATE Colorado

APPROVED [Signature] DATE JANUARY 1916
DESIGN 6-3176

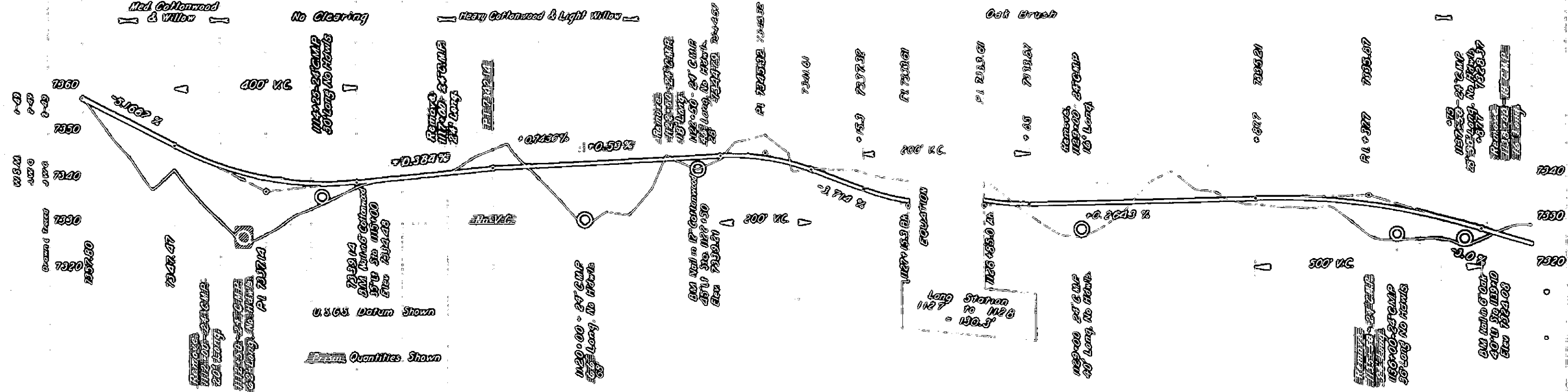






The Alignment and Grade as hereon shown are subject to adjustment.

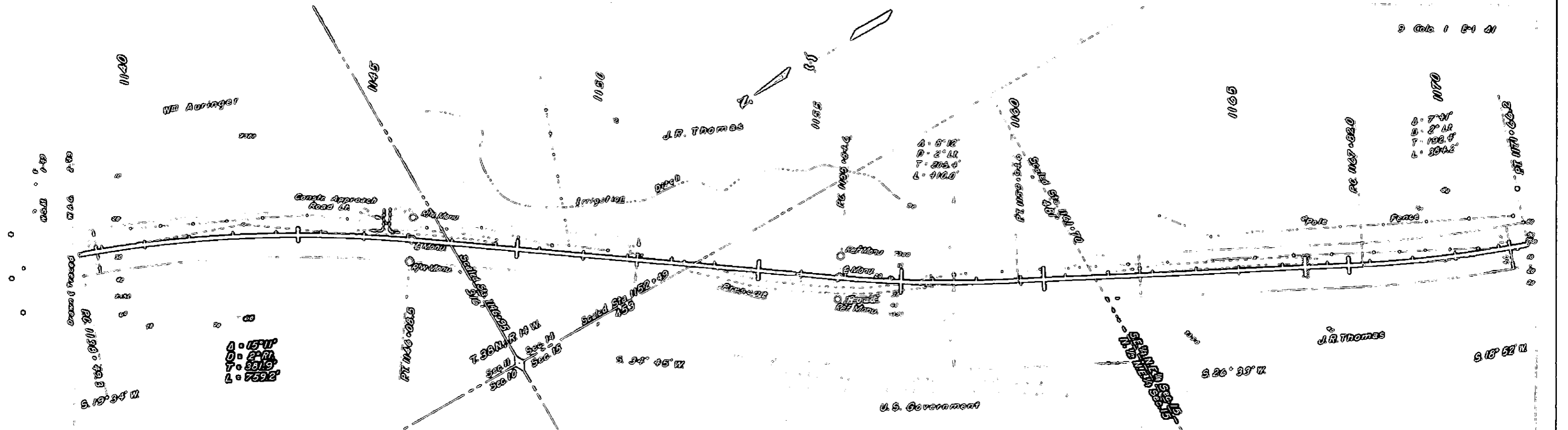
Med Cottonwood & Willow No Clearing Heavy Cottonwood & Light Willow Oak Brush



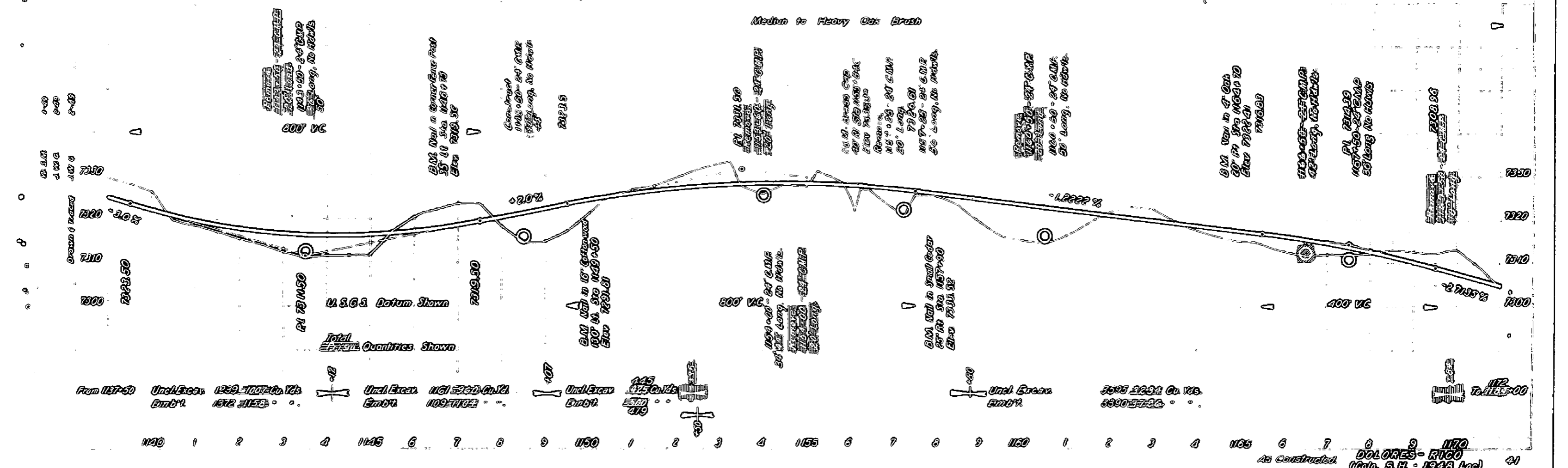
From 1090 + 75

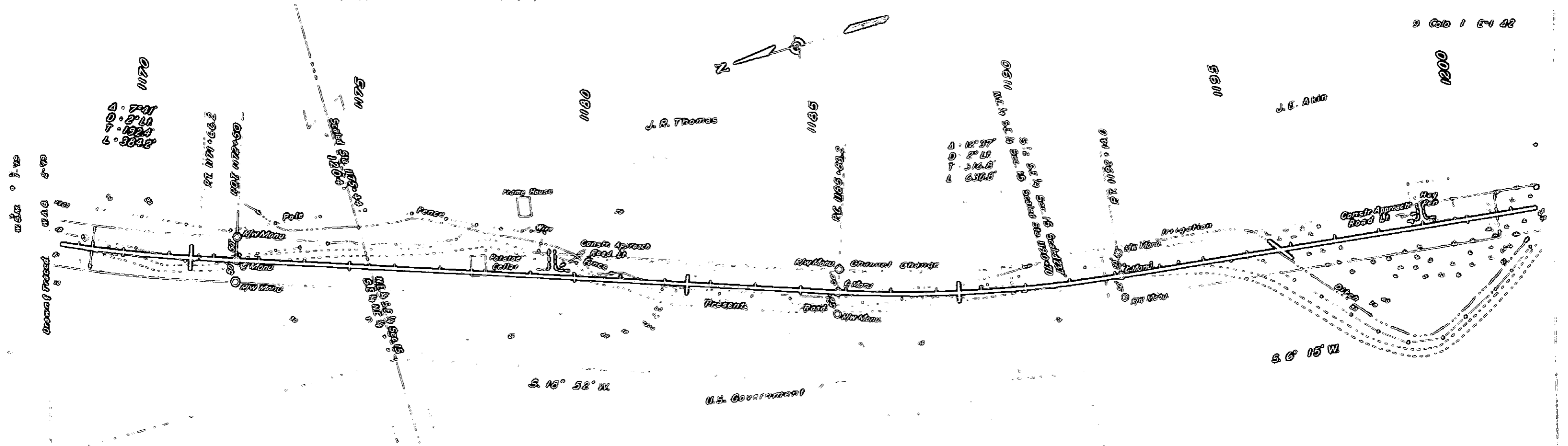
Unit Embankment

3.194
5904

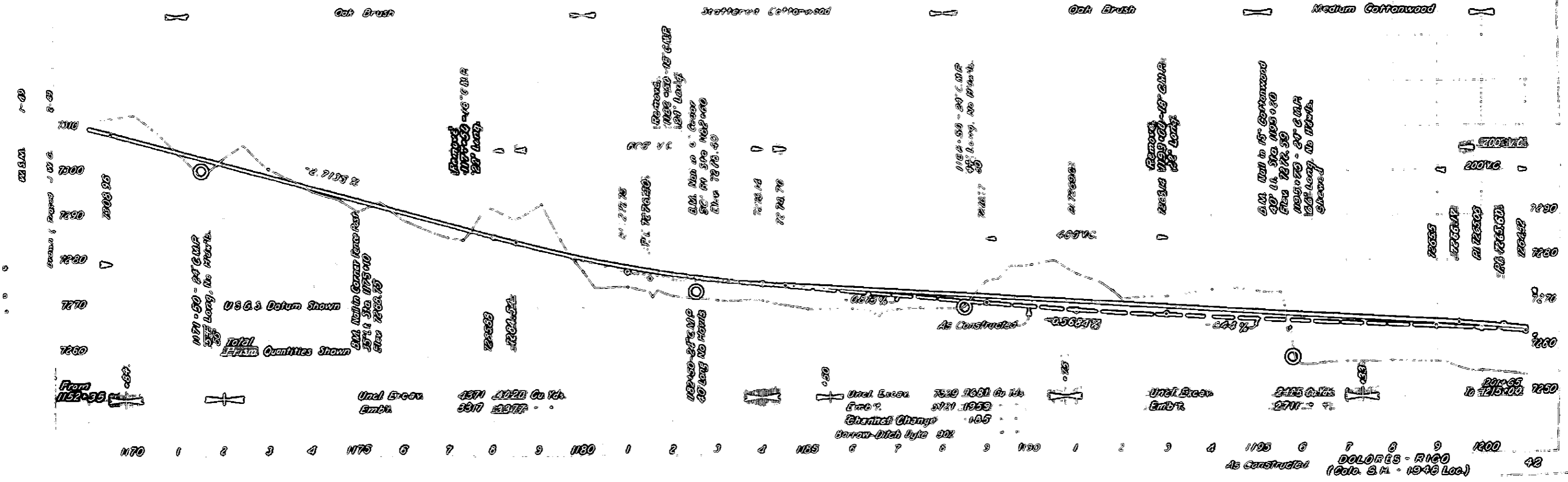


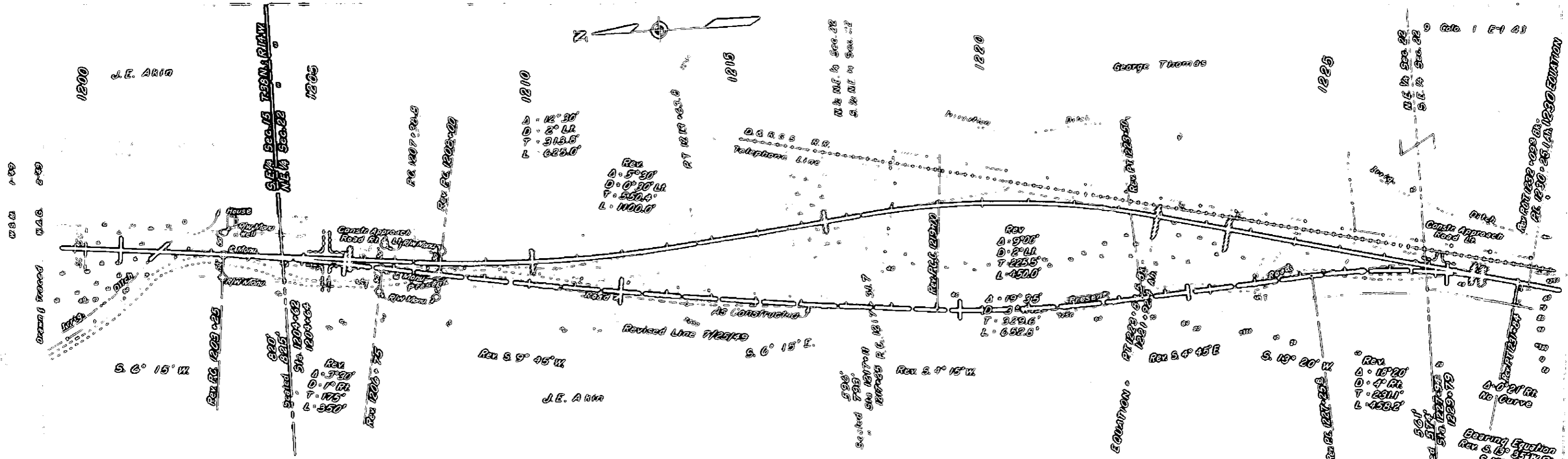
The Alignment and Grade as hereon shown are subject to adjustment.



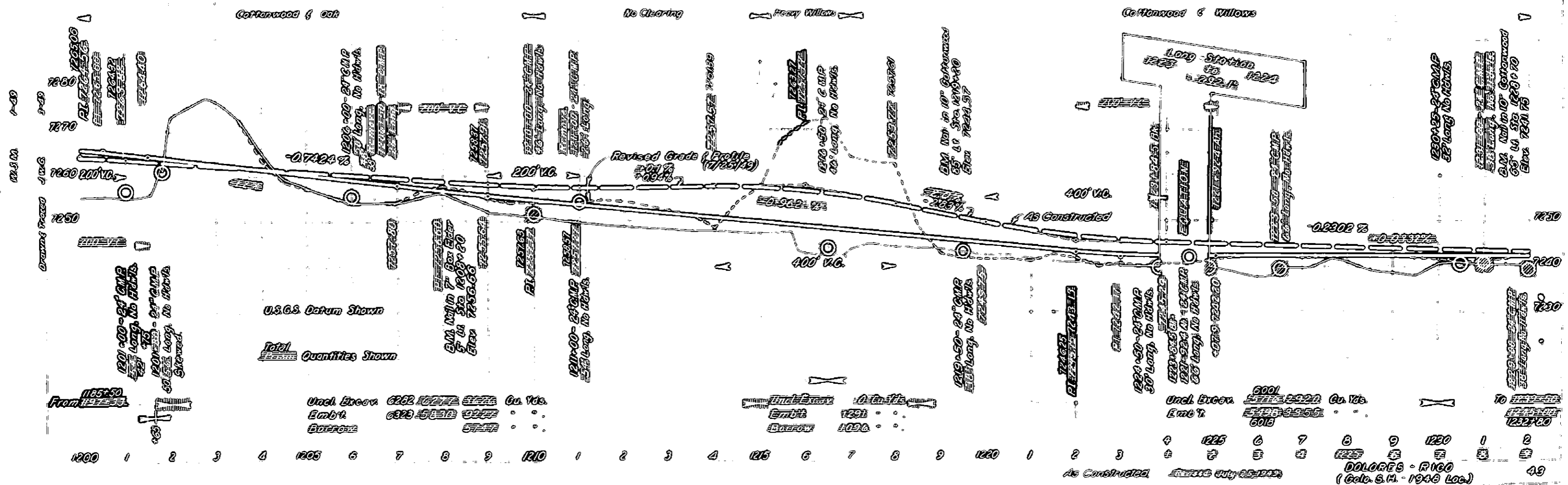


The Alignment and Grade as hereon shown are subject to adjustment.





The Alignment and Grade as hereon shown are subject to adjustment.



U.S.G.S. Datum Shown

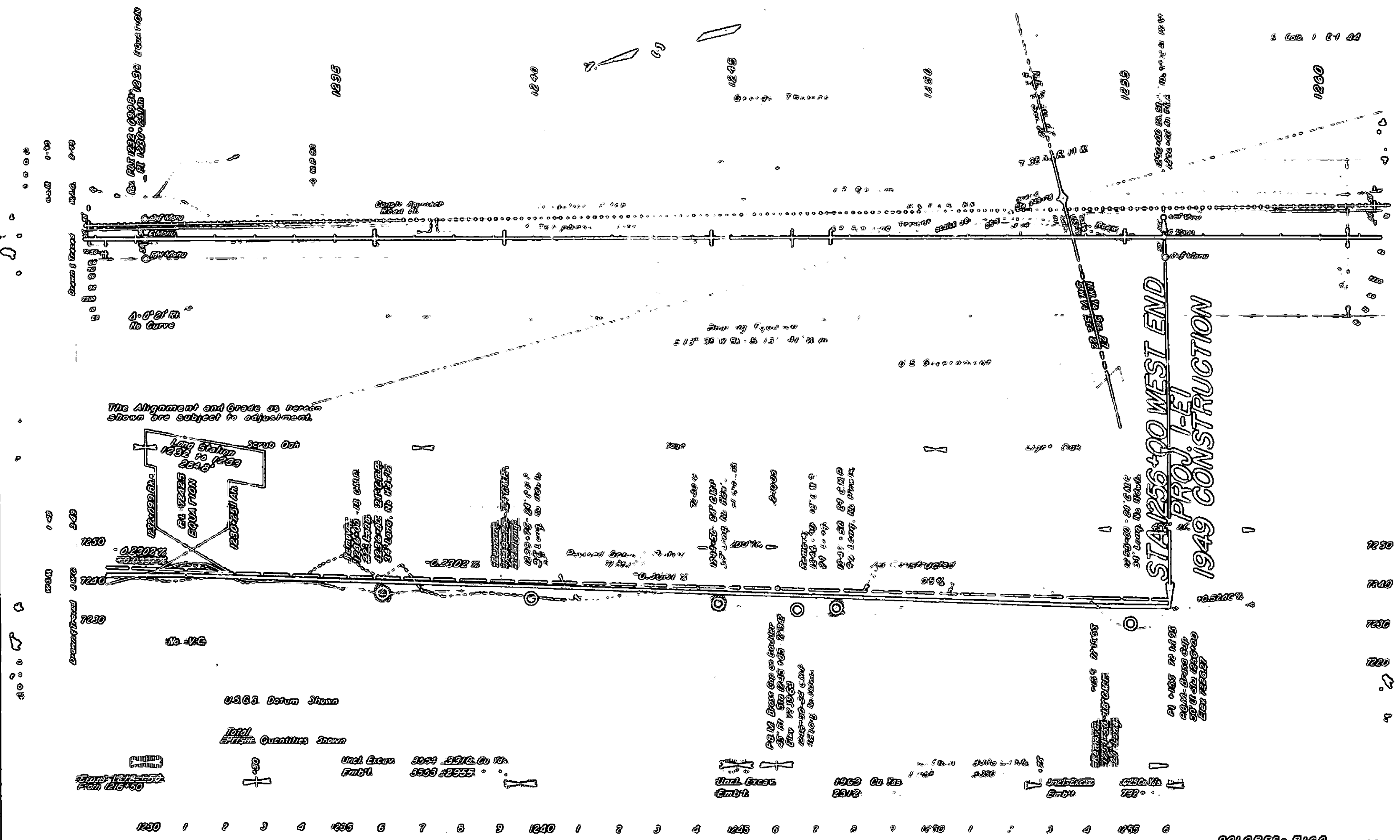
Total Quantities Shown

Und. Excav.	Emb't	Entrench.	Gu. Yds.
6202	5152	5277	
6202	5152	5277	
		5277	

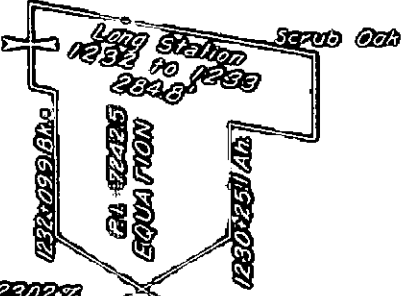
Und. Excav.	Emb't	Entrench.
1291		
1094		

Und. Excav.	Emb't	Gu. Yds.
6001	5277	
5277	5277	
6018		

1200 1 2 3 4 1205 6 7 8 9 1210 1 2 3 4 1215 6 7 8 9 1220 1 2 3 4 1225 6 7 8 9 1230 1 2
 As Constructed
 DOLORIS - RICO
 (Gold. S.M. - 1946 Loc.) 49



The Alignment and Grade as herein shown are subject to adjustment.



U.S.G.S. Datum Shown

Total
 EARTH Quantities Shown

From 1216+50
 From 1216+50

Uncl. Excav. 3564 2312 Cu Yd.
 Emb'd. 3563 2355 "

Uncl. Excav. 1969 On X's
 Emb'd. 2612 "

Uncl. Excav. 4230 1/2
 Emb'd. 782 "

1230 1 2 3 4 1235 6 7 8 9 1240 1 2 3 4 1245 6 7 8 9 1250 1 2 3 4 1255 6

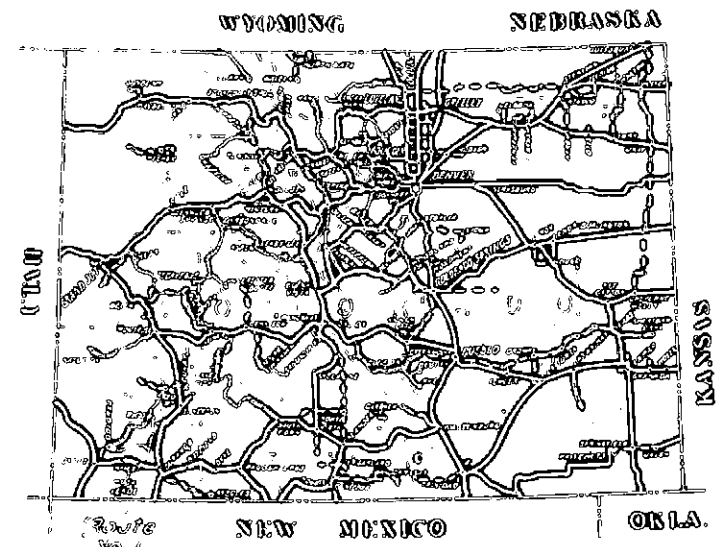
FEDERAL WORKS AGENCY
PUBLIC ROADS ADMINISTRATION

AS CONSTRUCTED
PROJECT FE1
ROUTE 1 DOLORES-RICO HIGHWAY
DOLORES TO A JUNCTION WITH STATE HIGHWAY NO 108
65 MILES CLASS 2
COLORADO FOREST HIGHWAY SYSTEM

SAN JUAN NAT'L. FOREST
DOLORES, MONTEZUMA & SAN MIGUEL CO'S
COLORADO

NO.	STATE	ROUTE OR DIST.	SECTION	SHEET NO.	TOTAL SHEETS
1	CO	1	1	1	1

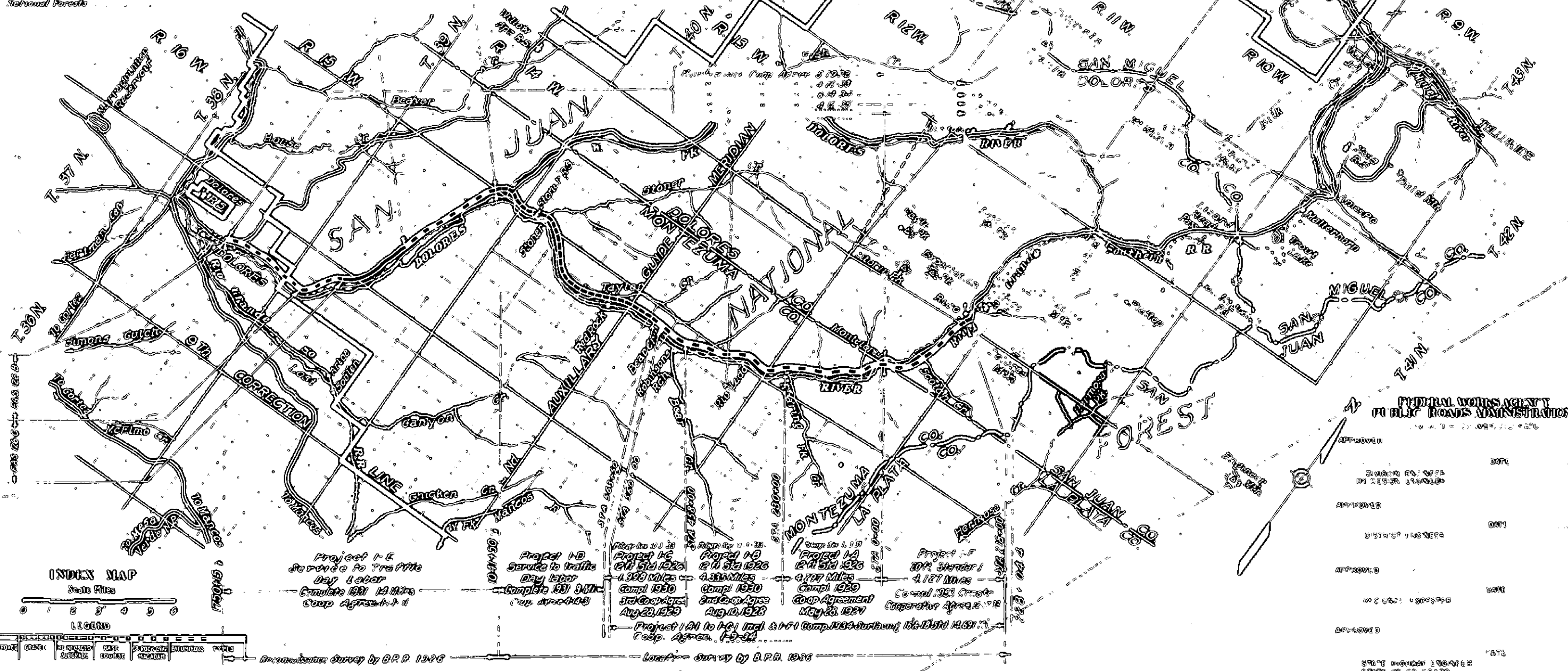
SHEET NO.	DESCRIPTION	DATE
1	Title Sheet	
2	Typical Sections	
3	Summary	
4	Plan & Profile	04-06-1926-00
5	Sta. & M. Marks for S.M.P. Cont.	
6	Sta. Maintenance Structures	
7	Sta. Maintenance Posts	
8	Typical Construction Signs	
9	Sta. Barbed Wire Fence	04-06-1926-00
10	Cross Sections	04-06-1926-00
11	Channel Channel	04-06-1926-00



Federal and Highway System
Primary
Secondary

Forest Highway System
National Forests

Project FE1
Complete 1929
Grading 20000
Surfacing 100
Coop. Agrees



Project FE1
Service to Traffic
Day Labor
Complete 1929
Coop. Agrees

Project FE2
Service to Traffic
Day Labor
Complete 1929
Coop. Agrees

Project FE3
12.4 Miles
Complete 1929
Coop. Agrees

Project FE4
12.4 Miles
Complete 1929
Coop. Agrees

Project FE5
4.187 Miles
Complete 1929
Coop. Agrees

Project FE1 to FE5
Total 65.71 Miles
Complete 1929
Coop. Agrees

FEDERAL WORKS AGENCY
PUBLIC ROADS ADMINISTRATION

APPROVED

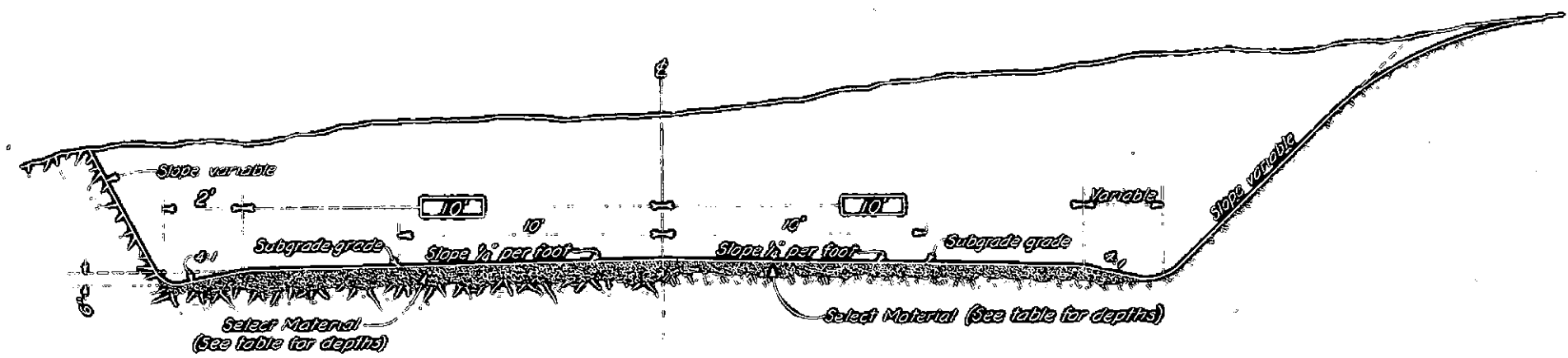
APPROVED

APPROVED

APPROVED

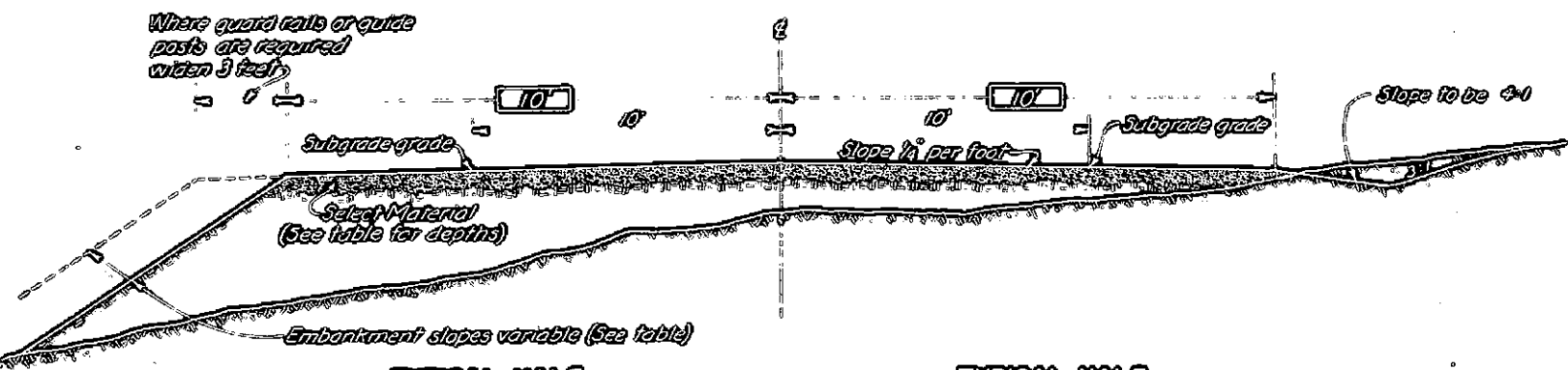
APPROVED

Survey by B.P.A. 1926



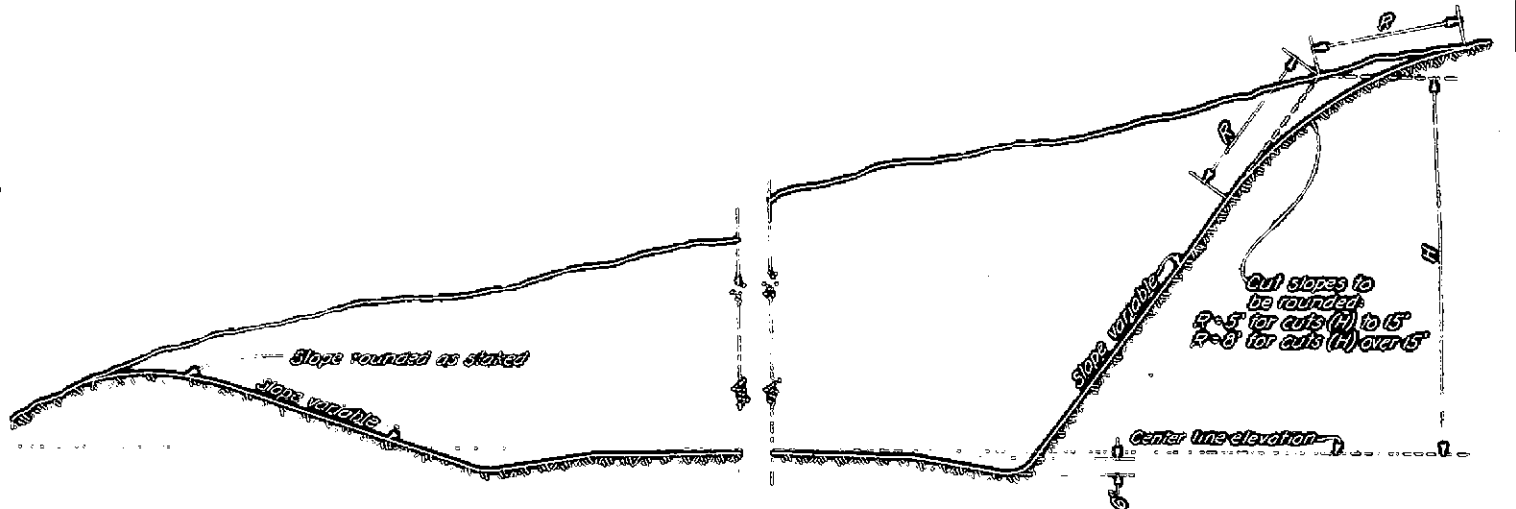
TYPICAL HALF CUT SECTION IN ROCK

TYPICAL HALF CUT SECTION IN COMMON

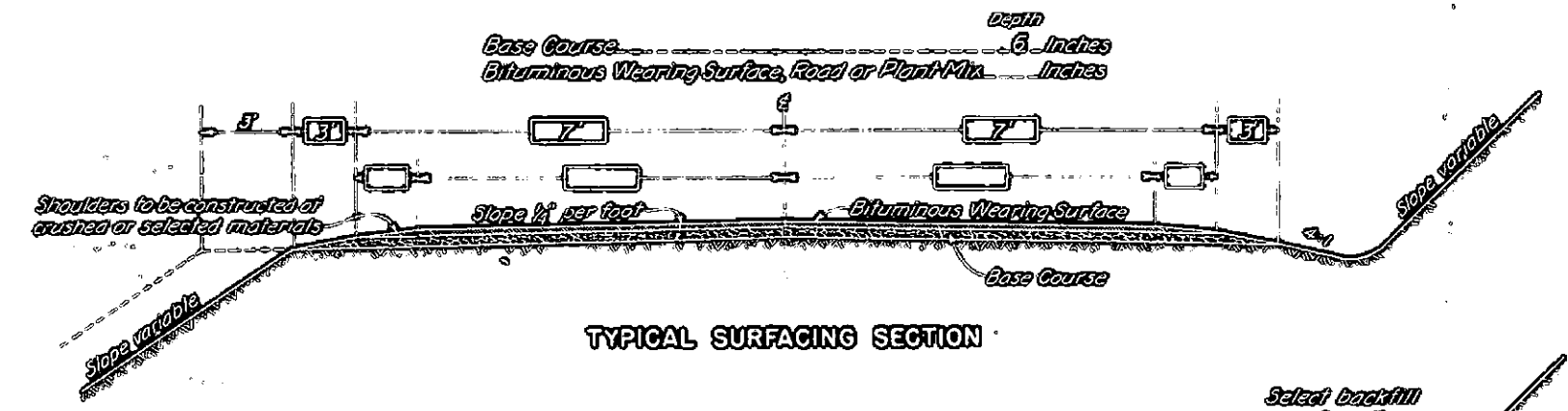


TYPICAL HALF EMBANKMENT SECTION

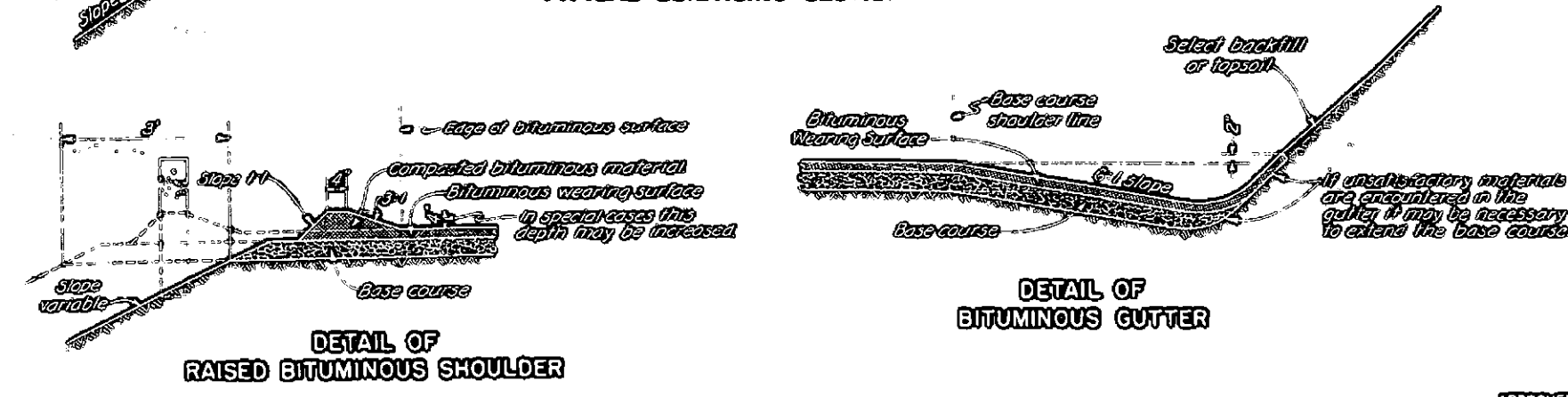
TYPICAL HALF SHALLOW EMBANKMENT SECTION



SLOPE TREATMENT IN EARTH CUTS



TYPICAL SURFACING SECTION



DETAIL OF RAISED BITUMINOUS SHOULDER

DETAIL OF BITUMINOUS GUTTER

GENERAL NOTES

When excess is specified in the contract and sufficient material is found in the roadway excavation the right is reserved to increase the amount of classified excavation and reduce the unclassified excavation for borrow. When additional material is needed for completing embankments, stabilizing the subgrade, or for selected sections or topping it may be secured by uniformly widening from or ditching out or flattening out slopes where satisfactory material is available. The slopes of the ends of all cuts shall be flattened and flared to improve appearance. Parrow ditches shall be constructed on approximate one per cent grades following the ground contour and where possible shall be so constructed that the direction of flow will be away from the roadway.

Topsail shall be covered and either placed in stockpiles or spread over cut and embankment slopes as directed and in accordance with the specifications. Roadway ditches of the ends of cuts shall be constructed so as to carry the flow away from the adjacent embankment slopes. Embankment slopes shall be uniformly warped between one rate of slope and another the transition shall cover a distance of not less than thirty feet. Subgrade shall be on the surface of sub-used graded road and located far from center line of tangents and insides of curves. Public roads adjoining either ends of transition curves for highways shall be used to determine super-elevation and transition lengths (table I) and widening of curves (table II).

SELECT MATERIAL

STATION TO STATION	DEPTH

SLOPES

HEIGHT	EMBANKMENT SLOPE	EARTH CUT SLOPE	ROCK CUT SLOPE
0' to 3'	3:1	3:1	1:1
3' to 6'	4:1	5:1	1:1
6' to 12'	2:1	2:1	1:1
12' to 30'	1 1/2:1	1 1/2:1	1:1
30' up	1:1	1:1	1:1

When field conditions indicate the need for slopes other than those indicated above they shall be constructed as stated by the Engineer in cases where the height of cuts are in excess of 30 feet and in material which grades readily use slopes of 1 1/2 to 1 or flatter if practical.

FEDERAL WORKS AGENCY
PUBLIC ROADS ADMINISTRATION
DIVISION NO. 9 DENVER, COLORADO

TYPICAL CROSS SECTIONS
NATIONAL FOREST & PARK HIGHWAYS

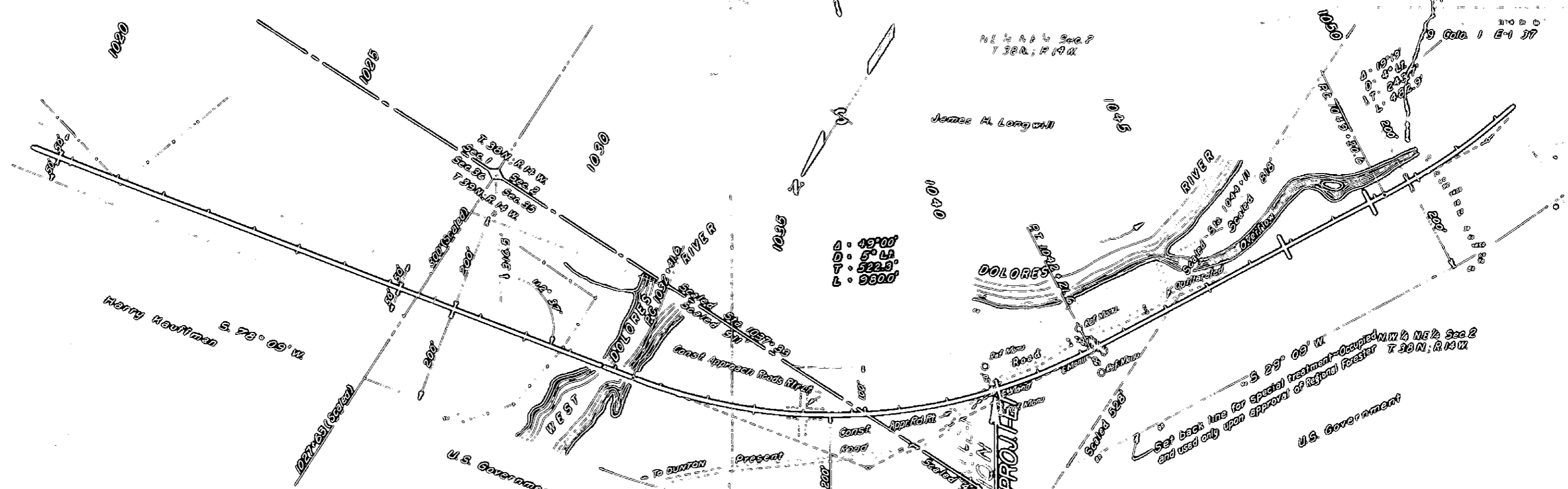
(GRADED 20' Shoulder to Shoulder)
BASE COURSE 1 1/2"
BITUMINOUS WEARING SURFACE 1"
DIVISION No. 9 STANDARD ROADBED:
PROJECT Colo. 1-51 Dolores - Rito
NATIONAL Forest & San Juan
COUNTY Montezuma
STATE Colorado

APPROVED [Signature] DATE JANUARY 1946

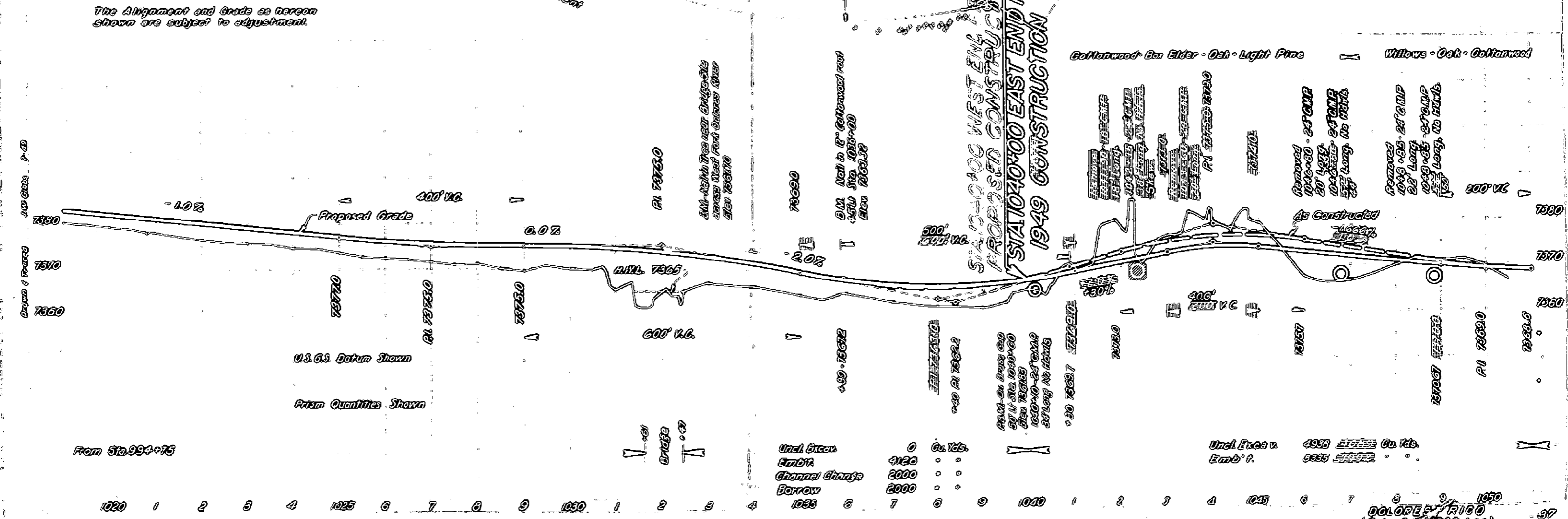
Notes - Also, Gato, 1
Sta. 1020+00 to Sta. 1030+00
Sheet No. 37

Drawn & Traced - M.A.G. 1-49

Drawn & Traced - P. 49



The Alignment and Grade as hereon shown are subject to adjustment.



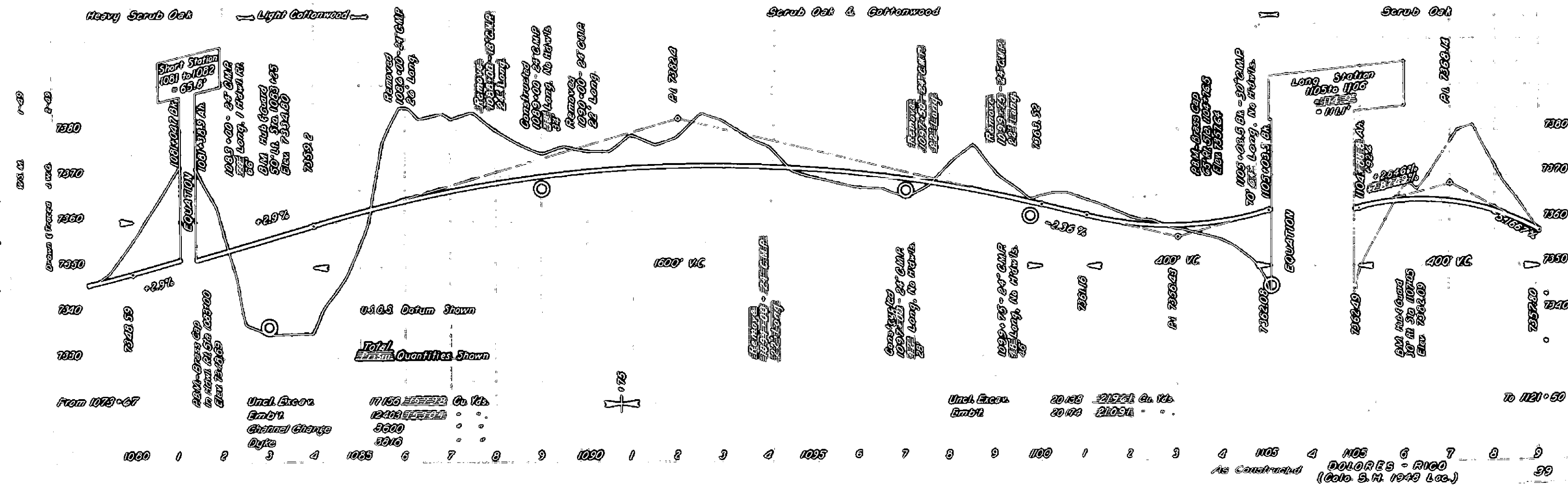
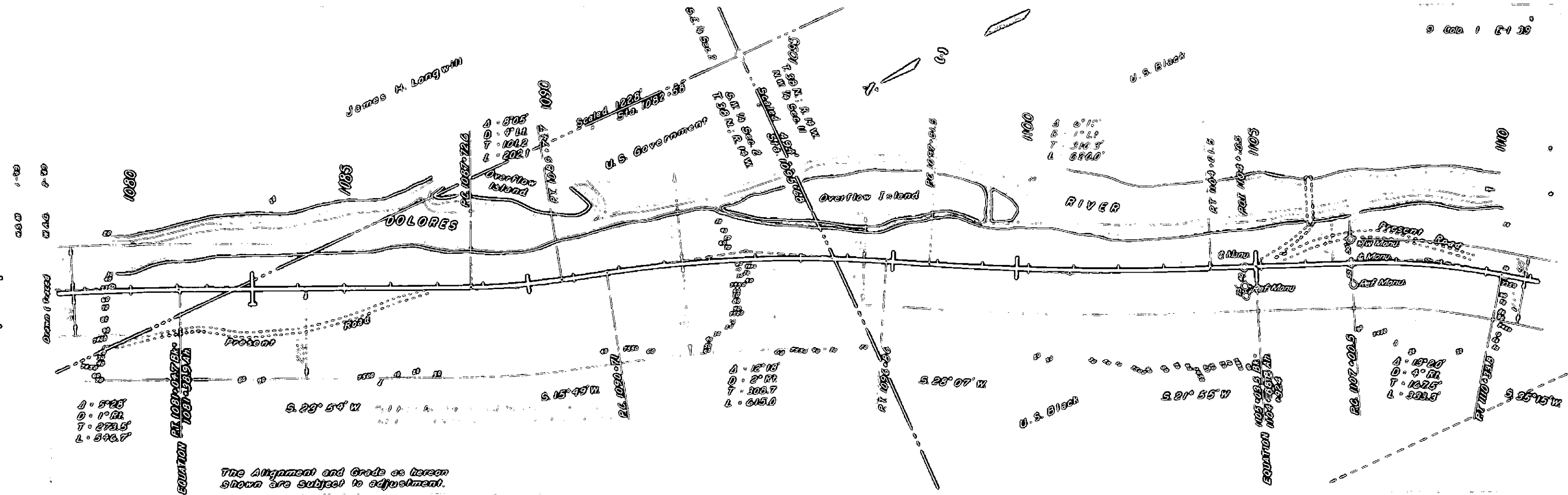
From Sta. 994+75

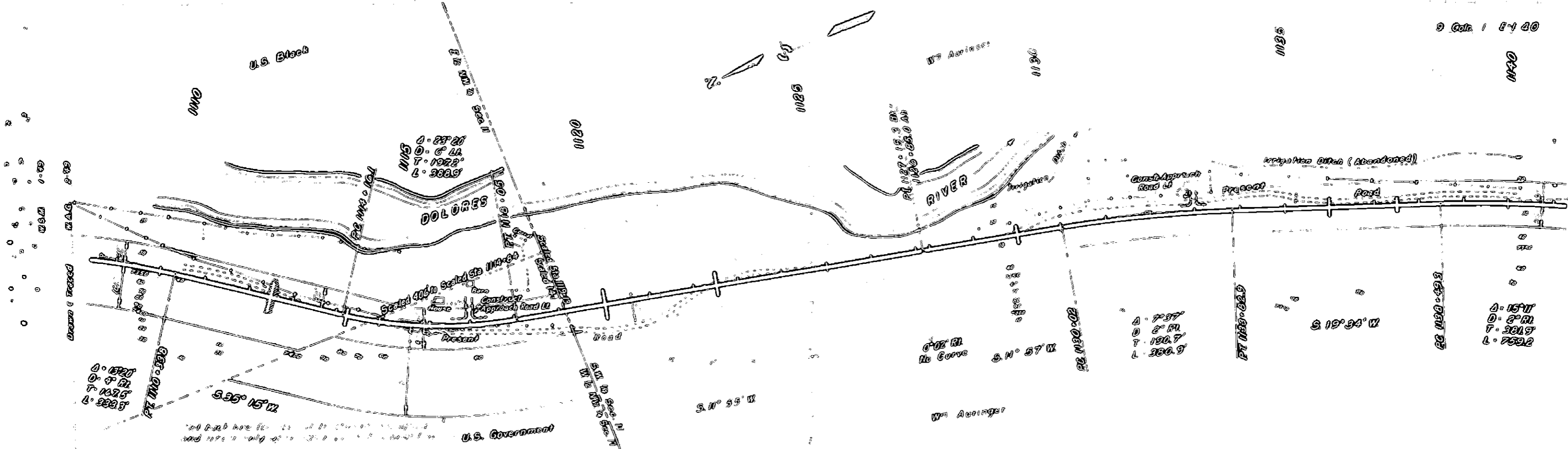
U.S.G.S. Datum Shown

Prism Quantities Shown

Uncl. Excav.	0	Cu. Yds.
Emb'd.	4126	"
Channel Change	2000	"
Borrow	2000	"

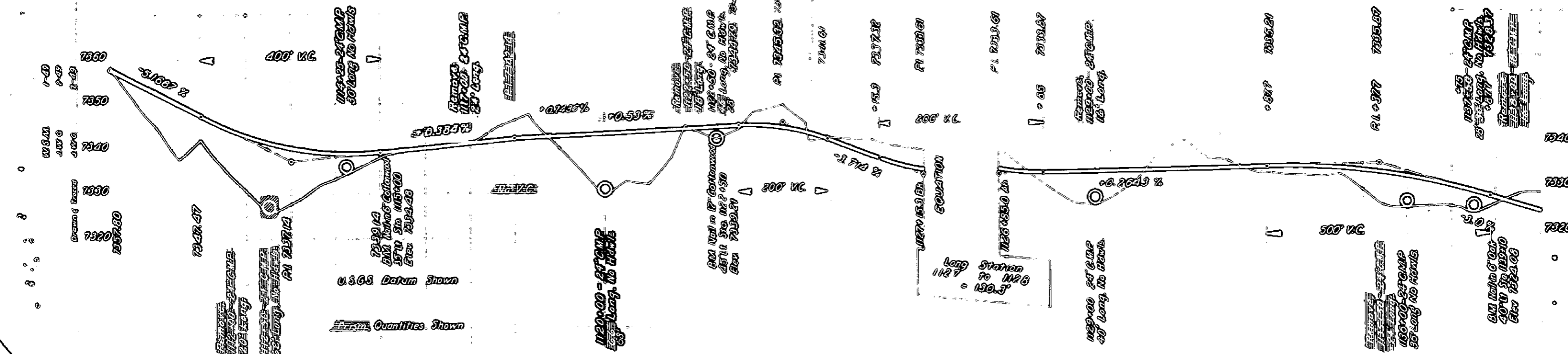
Uncl. Excav. v. Emb'd. f.	4938	Cu. Yds.
	5635	"





The Alignment and Grade as hereon shown are subject to adjustment.

Med. Cottonwood & Willow No Clearing Heavy Cottonwood & Light Willow Oak Brush

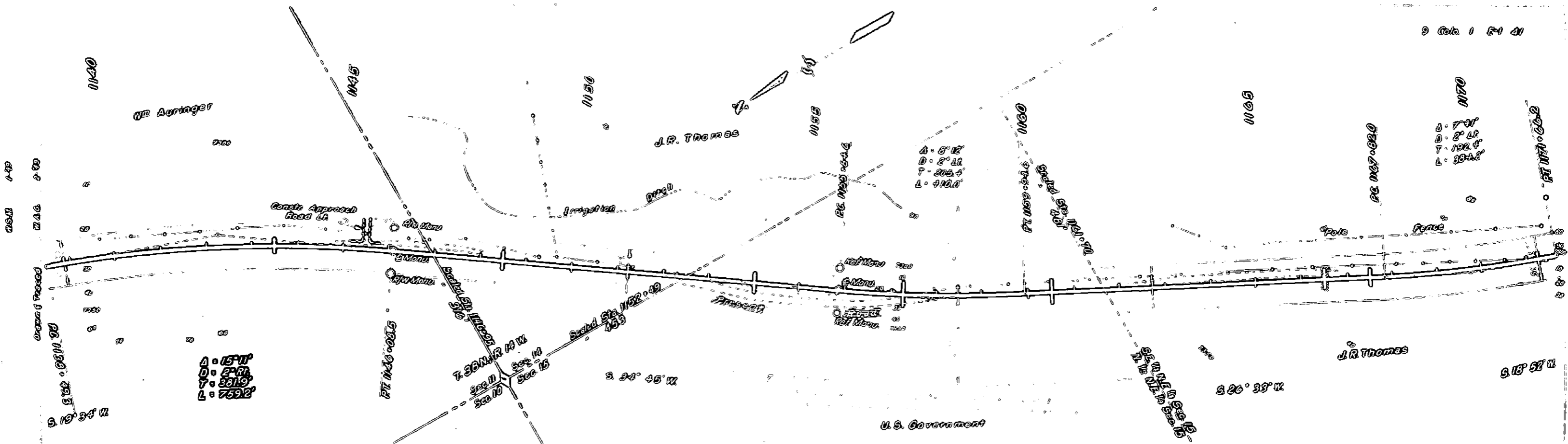


U.S.G.S Datum Shown

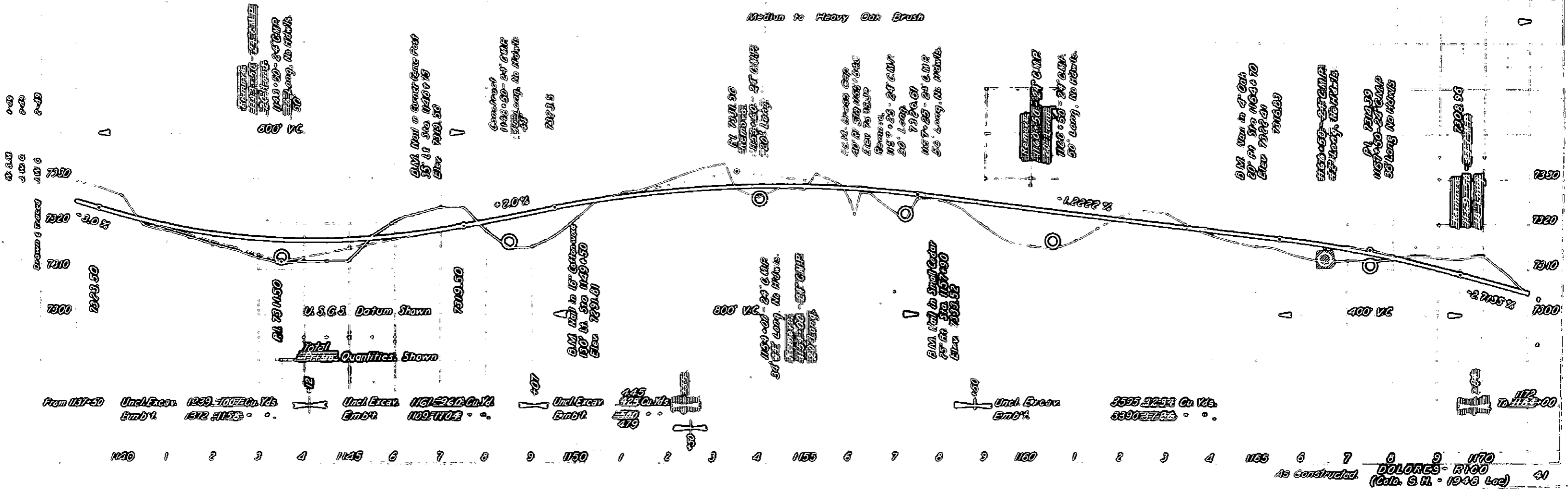
Quantities Shown

From 1020 + 75

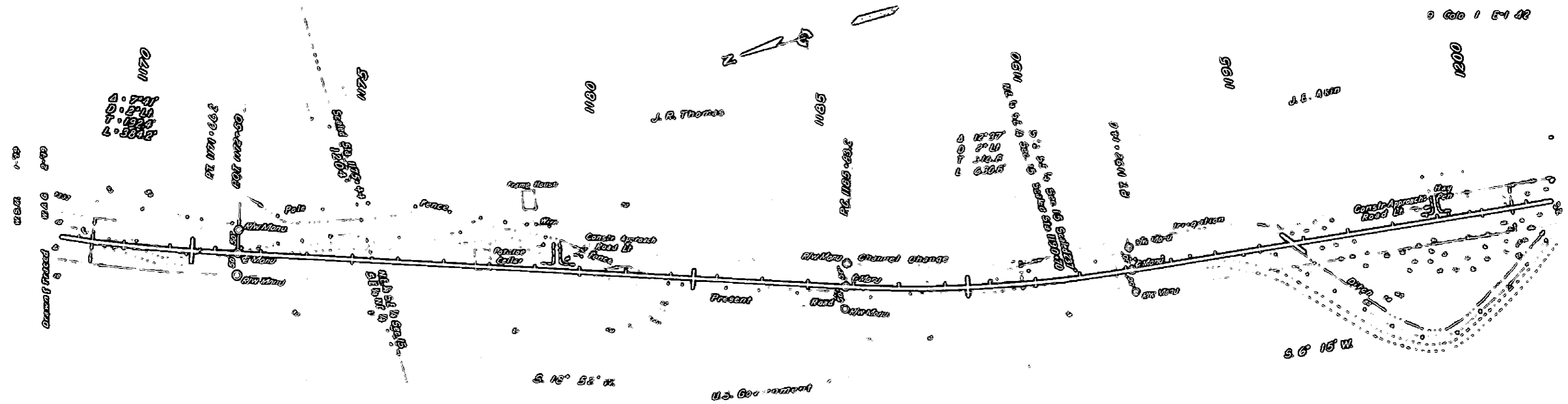
Unit Emb't. 3794 3715 Cu. Yds.
 3904 3719 "



The Alignment and Grade as hereon shown are subject to adjustment.

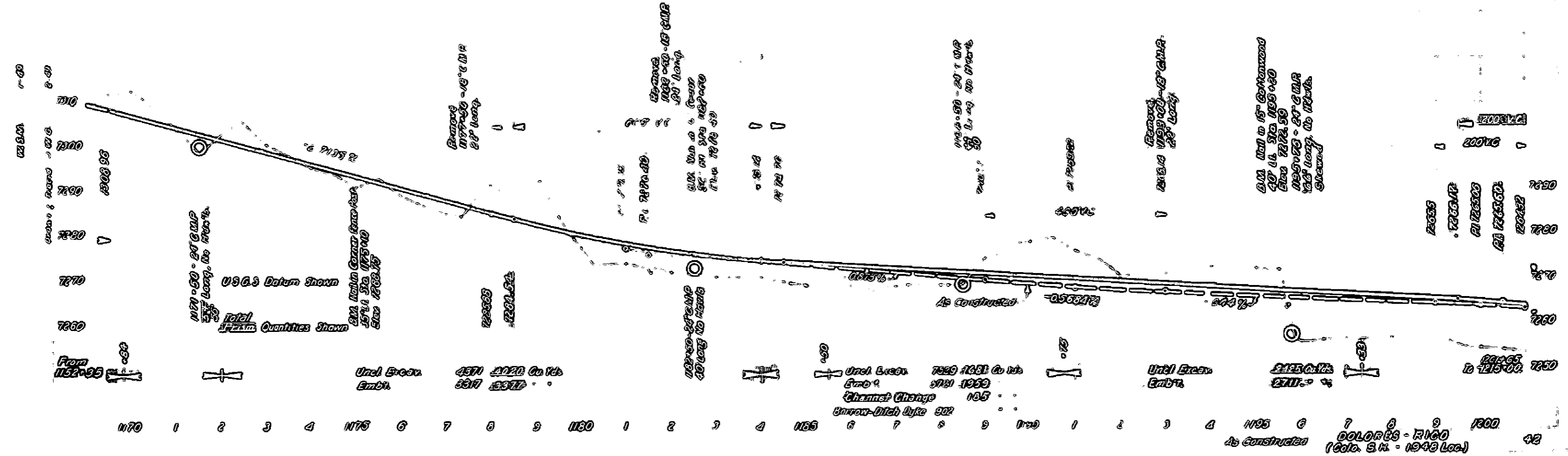


Station	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
From 1137+50																				
Uncl. Excav. Emb'd	1239	1007	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012	1012
Uncl. Excav. Emb'd	1161	2261	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109	1109
Uncl. Excav. Emb'd	3595	3252	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390	3390



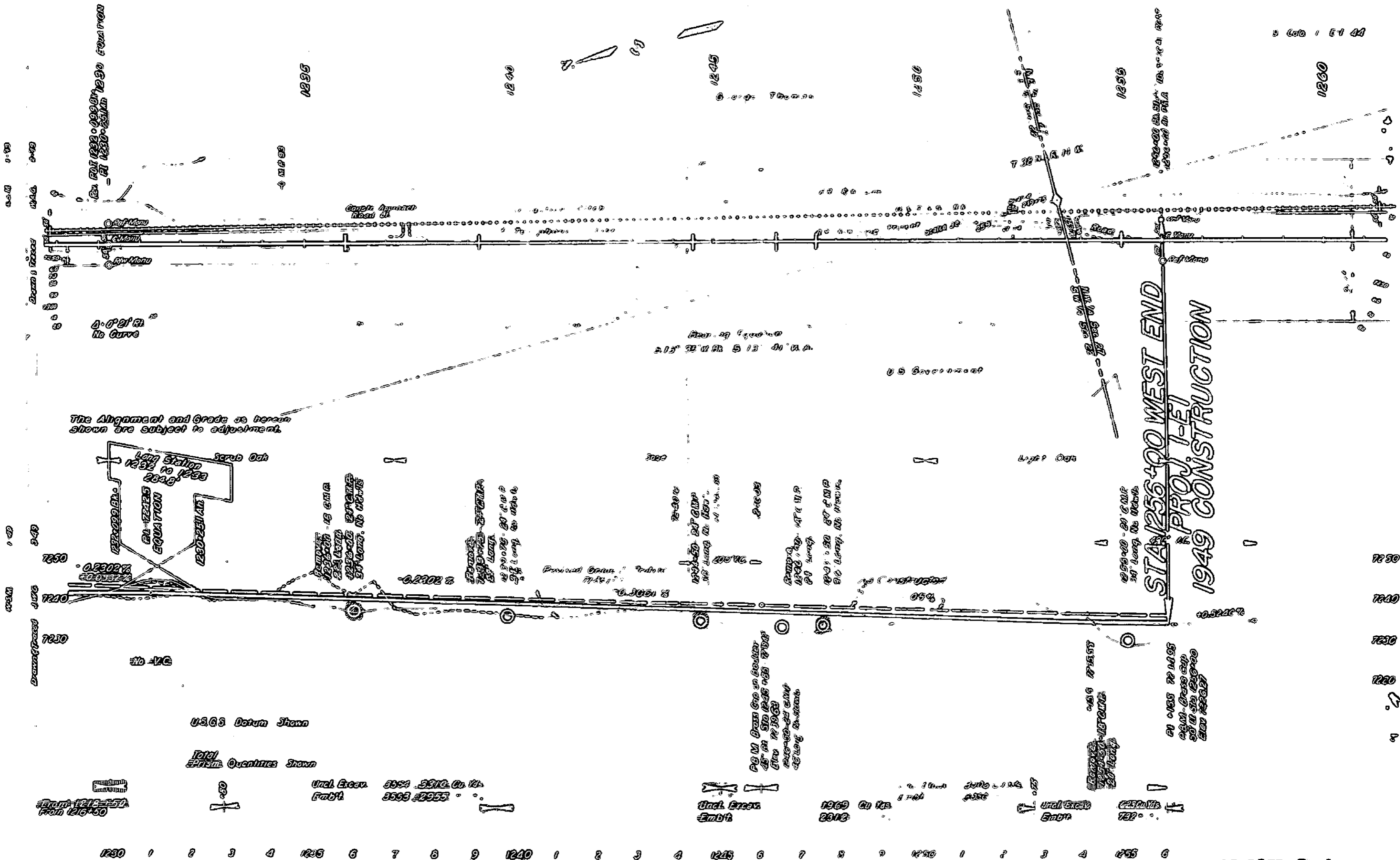
The Alignment and grade as shown are subject to adjustment.

Oak Brush Scattered Cottonwood Oak Brush Medium Cottonwood



1170	1	2	3	4	1175	6	7	8	9	1180	1	2	3	4	1185	6	7	8	9	1190	1	2	3	4	1195	6	7	8	9	1200
															Under Erosion Embankment 4571 4020 Cu Yds 3317 3317										Channel Change 185		Under Erosion Embankment 215 Cu Yds 215			
															1185-1200 - 24' C.M.P. 40' Long No Hauls															
															1170-1175 - 24' C.M.P. 55' Long, No Hauls															
															1180-1185 - 16' C.M.P. 55' Long															
															1185-1190 - 18' C.M.P. 55' Long															
															1190-1195 - 20' C.M.P. 55' Long, No Hauls															
															1195-1200 - 15' Cottonwood 40' 14' Sta. 1195+20 Elev. 7272.59 1195+20 - 24' C.M.P. 120' Long, No Hauls.															
															1175-1180 - 24' C.M.P. 55' Long, No Hauls.															
															1180-1185 - 24' C.M.P. 55' Long, No Hauls.															
															1185-1190 - 24' C.M.P. 55' Long, No Hauls.															
															1190-1195 - 24' C.M.P. 55' Long, No Hauls.															
															1195-1200 - 24' C.M.P. 55' Long, No Hauls.															

DOLORES - RICO
 (Colo. S.H. - 1948 Loc)



The Alignment and Grade as herein shown are subject to adjustment.

Long Station 1232 to 1259
 EQUATION
 EQUATION
 EQUATION

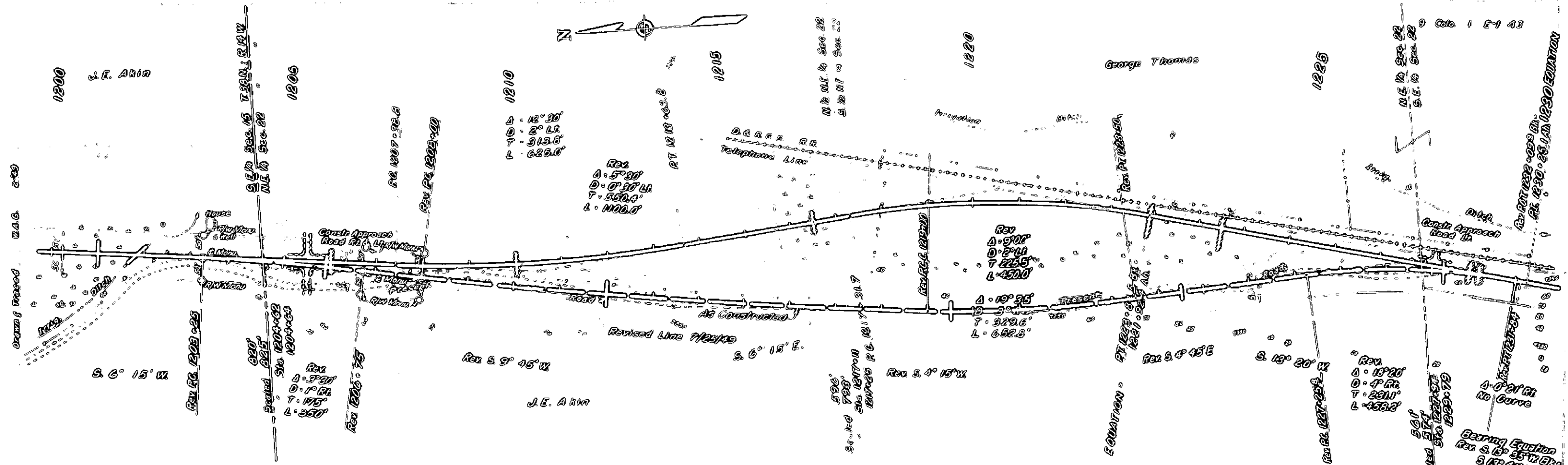
U.S.G.S Datum Shown

Total Embankment Quantities Shown

U.S.G.S Datum Shown
 1230 1235 1240 1245 1250 1255 1260

U.S.G.S Datum Shown
 1230 1235 1240 1245 1250 1255 1260

U.S.G.S Datum Shown
 1230 1235 1240 1245 1250 1255 1260



The Alignment and Grade as herein shown are subject to adjustment.

