



COLORADO
Department of
Transportation

DOCUMENT SEPARATOR SHEET

REGION 5 – JUNE 2017 CONVERSION

To be placed at the beginning of each separator sheet.



r500001850

Description:

ROW Plans 11X17

Route # and Mile Points:

SH 291

Originating Office:

ROW/Survey

File Name:

NRM 248 J_ROW(.PDF)

Box Location:

29 of 38

CLAUDE H. FERRO, Mayor
WALLACE SCHOOLFIELD, JR., Tax Ass.
MAX DICKEY, Police Magistrate

Wm. E. BUSH, City Attorney
C. L. GLENN, St. & Water Com.
HOWARD SWEDDON, City Eng.

E. REX FULLER, City Physician
A. A. BAILEY, City Marshal
WARREN L. BRITTON, Fire Chief

CITY OF SALIDA

SALIDA, COLORADO

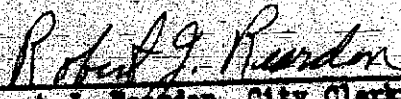
BOARD OF ALDERMEN

WM. J. BEAUREGARD
PHIL BOULEY
F. E. MCCARTHY

ALEX MCKENZIE
J. S. HANEY
G. H. SMITH

ROBERT J. REARDON, City Clerk

I, Robert J. Reardon, City Clerk of the City of Salida, Colorado, do hereby certify that the above and foregoing is a true and correct copy of the resolution adopted by the City Council of the City of Salida, Colorado, at a regular meeting of said City Council held April 2, 1934, and adjourned to April 3, 1934.


Robert J. Reardon, City Clerk

(S-E-A-L)

RESOLUTION

Passed by the Board Council of City of Salida, Colorado,

on September 4th, 1934

WHEREAS, It appears that a certain street or certain streets within the corporate limits of the City of Salida form necessary or convenient links for carrying State Highway No. 15 through said town, and

WHEREAS, It is mutually desired by the officials of said Town and the officials of the State Highway Department that the said connecting link be declared a State Highway in accordance with the provisions of the State Highway Act, (Session Laws of Colorado, 1921, Chapter 136, Section 23), therefore

BE IT RESOLVED, That we do hereby declare as a part of State Highway No. 15 the following street or streets, and agree that the same shall be subject to the rules and regulations of the State Highway Department:

Beginning at the North City Limits of Salida at the intersection of Grand Avenue and First Street; thence via First Street to Oak Street; thence via Oak Street to the South City Limits.

as shown on the sketch hereto attached.

BE IT FURTHER RESOLVED, That this Council agrees not to authorize or permit the installation of signs, semaphores or signals which will give any local intersecting street a status outranking the State Highway so declared; not to authorize or permit any construction or encroachment on the right of way; not to establish any curb lines nor cause or permit any curbing to be constructed on the highway with the curb lines less than thirty-eight (38) feet apart; not to pass, without first securing approval of the State Highway Department, any ordinance regulating the speed of vehicles or traffic on the said Highway; and to require that all parking of vehicles on said highway be made parallel to the right of way side lines.

The written approval of this action by the State Highway Engineer shall constitute an agreement between the parties in accordance with the provisions of Section 23, Chapter 136, Session Laws of 1921, and shall be binding upon the successors of both parties.

Passed this fourth day of September, 1934 at a regular meeting of the council.

(SEAL)

Attest: Robert J. Reardon
Clerk.

C. M. Terna
Mayor.

Recommended for approval:

James Montgomery 9/11/34
Division Engineer.

Assistant Engineer.

Approved: Sept 12, 1934
Charles Vail
S. State Highway Engineer

(Execute in duplicate, one copy to be returned to Town Clerk, when approved.)
(Revised January 15, 1931.)

AGREEMENT BY MUNICIPALITY OR COUNTY
TO MAINTAIN HIGHWAYS IMPROVED UNDER
NATIONAL INDUSTRIAL RECOVERY ACT.

MR. HENRY
FILE
APR 10 1934
15 18 19 20 21 22 23 24 25 26 27 28 29 30

Salida

The Municipality ~~County~~ of _____
State of Colorado, hereinafter referred to as Municipality ~~County~~, and the Colo-
rado State Highway Department, hereinafter referred to as The Highway Department
hereby agree as follows:

1. That the Highway Department will submit a project for the improvement
of approx. 1.2 miles of State Highway No. 15
in Salida on Oak St.; South city limits to Wood Ave. thence
via ^{West Street} First St. to C St.; also ^{East} C St. from R.R. tracks to Grand Ave.,
thence via Grand Ave. to northerly city limits
and to be designated as Colorado Project No. U.S.P.W. NRW 24B-I

and will recommend its approval by the Secretary of Agriculture for construc-
tion with funds apportioned to the State under Section 204 of the National In-
dustrial Recovery Act; subject, however, to the condition that the Municipality
~~County~~ shall provide for its proper maintenance after completion of its im-
provements.

2. That the said Municipality ~~County~~ hereby requests the Highway Depart-
ment to submit the aforementioned project with recommendation that it be ap-
proved by the Secretary of Agriculture, and agrees that if such project is
approved and constructed by the Highway Department and the Secretary of Agricul-
ture it, thereafter, at its own cost and expense, will maintain the project in
a manner satisfactory to them, or their authorized representatives, and will
make ample provision each year for such maintenance.

IN WITNESS WHEREOF, the parties have hereunto affixed their signatures,
the Municipality ~~County~~ on the _____
day of February, 1934, and the Highway Department on the
APR 10 1934 day of _____, 19____.

Note: Any official signing for
and on behalf of a municipality
or county should attach an ori-
ginal or authenticated copy of
order, resolution, ordinance, or
charter provision, or a citation
to statute, under the authority of
which this agreement is executed.
Each agreement should be executed
in duplicate and three certified
copies should be furnished for the
Bureau of Public Roads. Delete
the word Municipality or County
as appropriate.

City of Salidas Colo.
Municipality ~~County~~

By Chas. Kern
Mayor

COLORADO STATE HIGHWAY DEPARTMENT
By Chas. D. Vail
CHAS. D. VAIL
State Highway Engineer

By _____

C. R. BROWN, TREASURER
W. L. BROWN, CHIEF OF POLICE
W. L. BROWN, POLICE SUPERVISOR

W. E. RUSH, CITY ATTORNEY
C. L. GLENN, ST. & WATER COM.
HOWARD SWEDDON, CITY ENGR.

C. REX FULLER, CITY PHYSICIAN
A. A. BAILEY, CITY MARSHAL
WARREN L. BRITTON, FIRE CHIEF

CITY OF SALIDA

SALIDA, COLORADO

BOARD OF ALDERMEN

W. L. BEAUREGARD
P. H. BOGGER
F. E. MCCARTHY

ALEX MCKENZIE
J. S. RANEY
G. H. SMITH

ROBERT J. BEARDON, CITY CLERK

Whereas, it is the desire of the City Council of the City of Salida, Colorado, to empower C. M. Ferno, Mayor of the City of Salida, Colorado, to make, execute and deliver a contract known as "Agreement by Municipality or County to Maintain Highways Improved Under National Industrial Recovery Act" for and on behalf of the City of Salida, Colorado, a municipal corporation.

Therefore, be it resolved by the City Council of the City of Salida, Colorado; that the said C. M. Ferno, Mayor of the City of Salida, Colorado, be and he is hereby empowered to make and execute the above named contract for and on behalf of the City of Salida, Colorado.

Passed, approved and adopted at a regular meeting of the City Council of the City of Salida, Colorado, held April 2, 1934, and adjourned to April 3, 1934.

C. M. Ferno
C. M. Ferno, Mayor

Attest:

Robert J. Beardon
Robert J. Beardon, City Clerk

(2-2-34)

RESOLUTION BEING FILE

Project No. NRM 248-J

Resolution previously in this file declaring

(see below) Street in town of

Salida a portion of

State Highway No. 15, filed in Resolution

File 9-20-1939.

Resolution date Sept. 4, 1934 Street width 38 ft.

Beginning at North City Limits of Salida at the intersection of Grand Ave and First St.; thence via First St. to Oak St.; thence via Oak St. to South City Limits.

September 13, 1934

Mr. J. A. Elliott, Dist. Engr.
 Bureau of Public Roads
 Denver, Colorado

Dear Sir:

I am handing you herewith three certified copies of Resolution, passed by the City Council of Salida on September 4, 1934, dedicating First Street and Oak Street as a part of State Highway No. 15, which has been approved by Mr. Chas. D. Vail, State Highway Engineer, thus constituting an agreement in accordance with the provisions of Section 23, Chapter 136, Session Laws of 1921.

This is required in connection with Project MEM 248-J.

Yours very truly,

CHAS. D. VAIL
 State Highway Engineer

BY _____
 C. T. REEDY
 Senior Asst. Highway Engineer

CTR. PM
 ENC

cc to Mr. Maloney - with one executed copy of Resolution.

Mr. Orig: Please note, register and return.

COLORADO
STATE HIGHWAY DEPARTMENT
PLAN AND PROFILE OF PROPOSED
U. S. P. W. PROJECT N.R.M. 248-J
STATE HIGHWAY NO. **291**
CHAFFEE COUNTY

DATE	BY	REVISION
APR 25 1934	CHAFFEE COUNTY	ST. 248-J

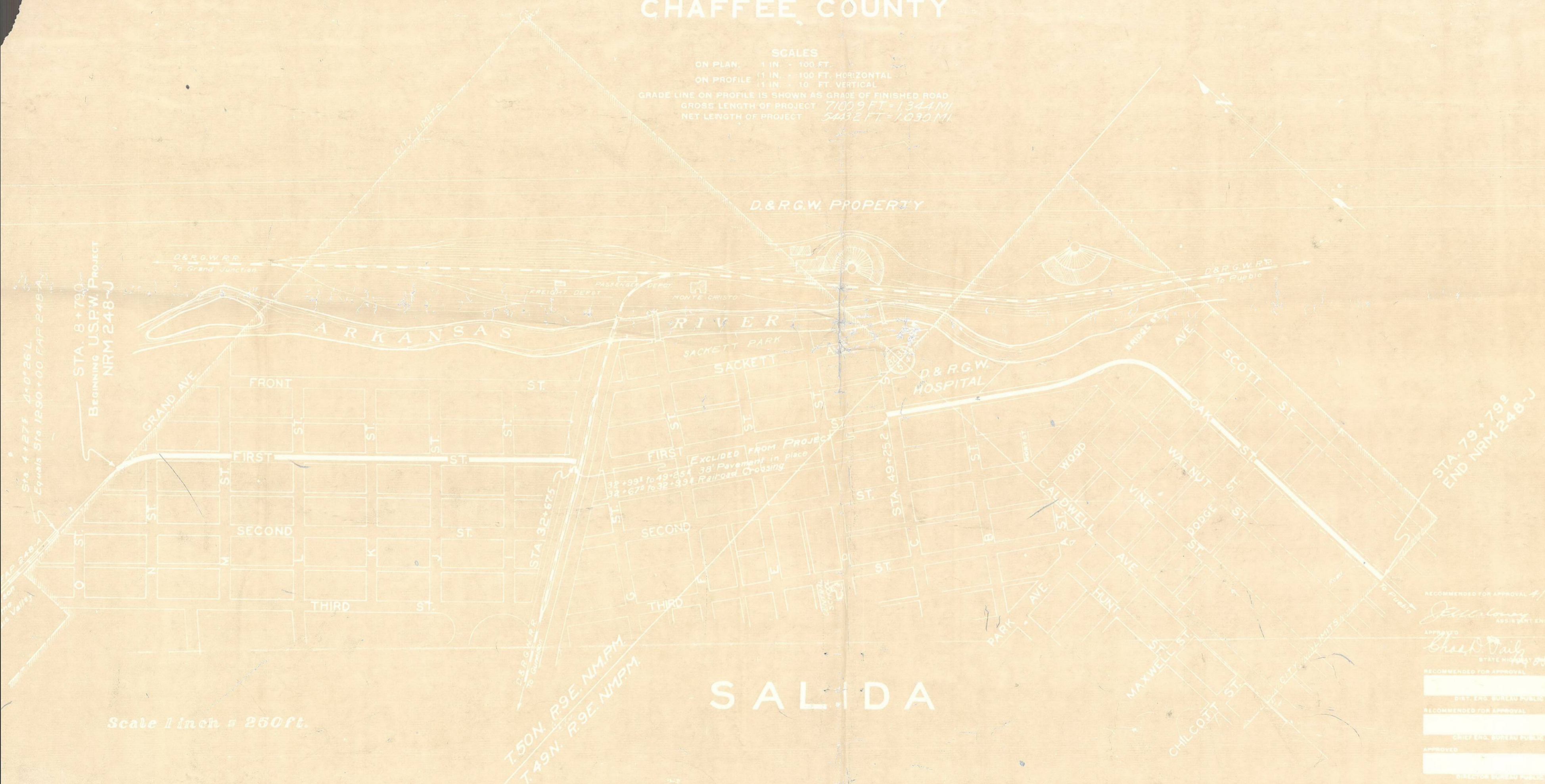
INDEX OF SHEETS

- 1. SHEET NO. 1, THIS SHEET AND SHEET MAP
- 2. TYPICAL CROSS SECTION AND SUMMARY OF QUANTITIES
- 3. STANDARD CONCRETE BOX CULVERTS, STD. NO. M-2-A
- 4. STANDARD METHODS FOR SUPERELEVATION AND WIDENING OF CURVES
- 5. STANDARD WIRE FENCES AND MARKER POSTS
- 6. ALIGNMENT PLANS AND PROFILES
- 7. ROADWAY CROSS SECTIONS

CONVENTIONAL SIGNS

- CENTER LINE OF SURVEY
- RIGHT OF WAY LINE
- TOWNSHIP LINE
- SECTION LINE
- QUARTER SECTION LINE
- CITY LIMITS
- PROPERTY LINES
- RAILROADS
- POLE LINES
- BARBED WIRE FENCE

SCALES
ON PLAN 1 IN. = 100 FT.
ON PROFILE 1 IN. = 100 FT. HORIZONTAL
1 IN. = 10 FT. VERTICAL
GRADE LINE ON PROFILE IS SHOWN AS GRADE OF FINISHED ROAD
GROSS LENGTH OF PROJECT 7100 FT. = 1.344 MI.
NET LENGTH OF PROJECT 5443 FT. = 1.030 MI.

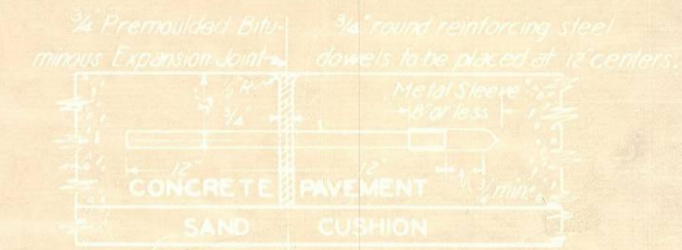


Scale 1 inch = 250 ft.

RECOMMENDED FOR APPROVAL 4/15/34
John A. ...
APPROVED
Chief D. ...
RECOMMENDED FOR APPROVAL
RECOMMENDED FOR APPROVAL
RECOMMENDED FOR APPROVAL
APPROVED
DIRECTOR BUREAU PUBLIC WORKS

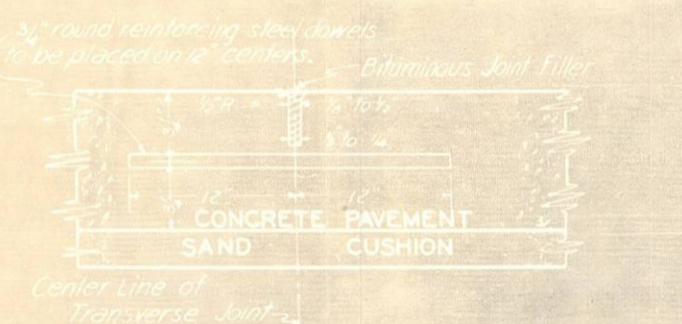
DETAILS OF JOINTS FOR CONCRETE PAVEMENT

DETAIL OF EXPANSION JOINT



1/4" round reinforcing steel dowels to be placed at 18" centers.
Metal sleeve 1/2" dia x 18" long.
1/4" holes formed in precast concrete slabs for the dowels.
1/4" precast aluminum expansion joints with steel dowels and centers are required to be constructed at right angles to the center line of the pavement.

DETAIL OF LONGITUDINAL JOINT



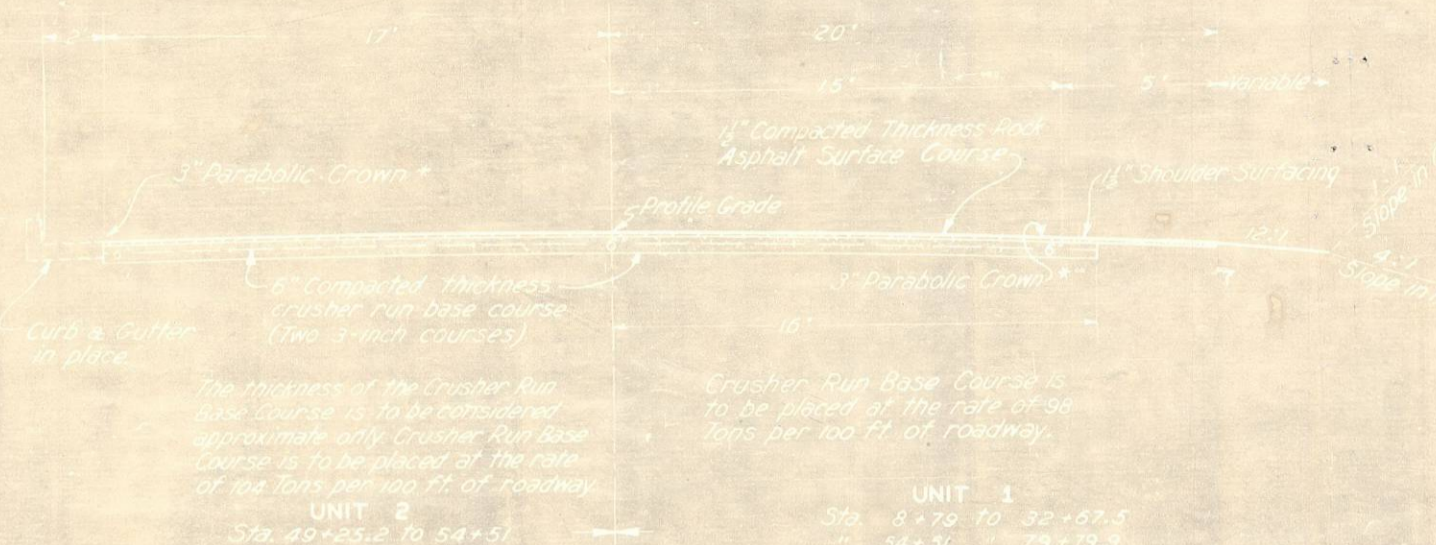
Two Transverse Weakened Plane Joints with steel dowels (6" dia) centers are to be constructed between expansion joints at intervals of 30' perpendicular to the center line of the pavement.
Longitudinal joint is built as this joint with the steel dowels omitted.

GENERAL NOTES

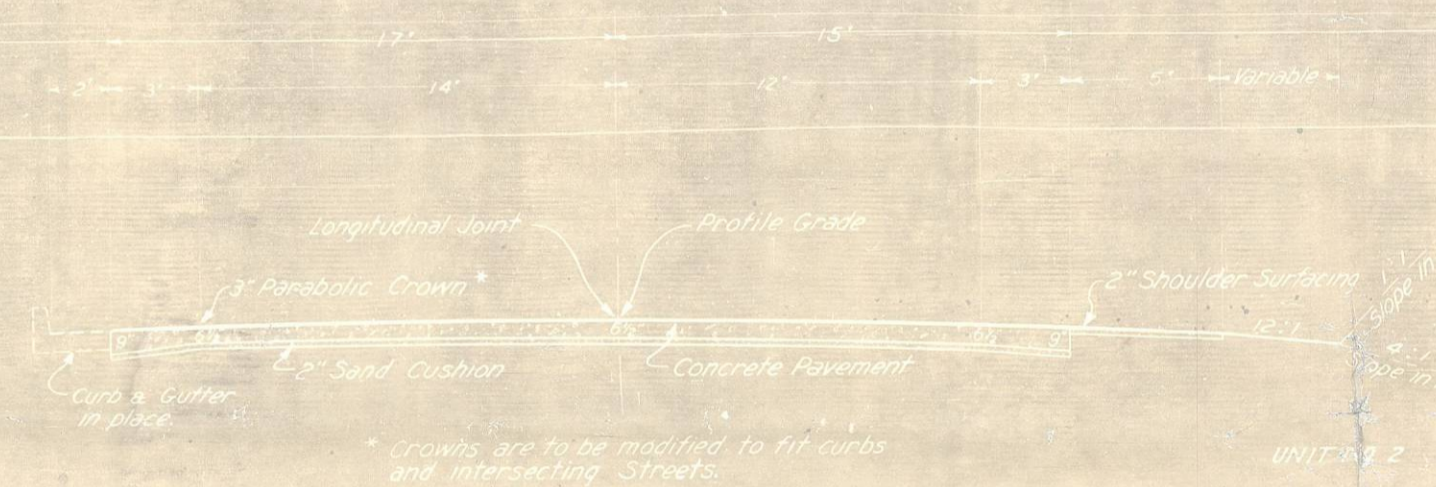
This project is to be constructed in conformity with the Standard Specifications of the Colorado State Highway Department, adopted January 1, 1930.
All quantities on preliminary plans are to be considered approximate only.
All roadway excavation required to construct the project is to be obtained as indicated on the plans. Quantities involved beyond the limits of the ditch as shown on the typical section either noted on the profile as "borrow" or on the tabulation of structures as "embankment" are to be classified and paid for as "unclassified excavation".
These quantities are to be staked as part of the original excavation at locations indicated on the plans. Slope stakes beyond the limits of the typical section are subject to change by the Engineer to fit embankment requirements actually met in construction.
All curves are to be super-elevated and widened as provided for by the Standard Super-elevation Sheet or as indicated on the plans.
All poles encroaching on the construction are to be moved by the owner.
When placed on the project where the new work lies along the present traveled road, the Contractor shall at his own expense prosecute construction in such a manner that traffic may readily pass over the road. Also, the Contractor shall maintain in safe condition and at his own expense, all temporary approaches.
Fire Hydrants and Valve Boxes to be moved or adjusted to new grade by the City of Salida.

TYPICAL CROSS SECTION OF IMPROVEMENT AND SUMMARY OF QUANTITIES

TYPICAL SECTION



ALTERNATE SECTION



TABULATION OF STRUCTURES

STATION	DESCRIPTION	GENERAL STRUC. NO.	CALCULATION	SHAFTS EXCAVATION	CROSS SECTION	DRAIN	CONCRETE	ADJUST. MANHOLE TO GRADE	MISCELLANEOUS
8+25	Project Marker								1 Project Marker
8+30	Remove & Rebuild Board Fence								Remove & Rebuild 220' Board Fence
8+35	20\"/>								
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8+80	20\"/>								
8+85	20\"/>								
8+90	20\"/>								
8+95	20\"/>								
9+00	Remove 16\"/>								
9+05	Remove Filling Station, Four Gas Pumps, Grease Rack, & Garages & Sheds and 2 Ash Pits								Remove Structures Asst. by State Forces
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28+35	20\"/>								
28+40	20\"/>								
28+45	20\"/>								
28+50	20\"/>								
28+55	20\"/>								
28+60	20\"/>								
28+65	20\"/>								
28+70	20\"/>								
28+75	20\"/>								
28+80	20\"/>								
28+85	20\"/>								
28+90	20\"/>								
28+95	20\"/>								
29+00	20\"/>								
29+05	20\"/>								
29+10	20\"/>								
29+15	20\"/>								
29+20	20\"/>								
29+25	20\"/>								
29+30	20\"/>								
29+35	20\"/>								
29+40	20\"/>								
29+45	20\"/>								
29+50	20\"/>								
29+55	20\"/>								
29+60	20\"/>								
29+65	20\"/>								
29+70	20\"/>								
29+75	20\"/>								
29+80									

NW 4 SEC. 32

SE 4 SEC. 32 T. 50N R. 9E N.M.P.M.

PER ROAD	STATE	NO. OF	SHEET	TOTAL
NO.	NO.	PIERS	NO.	PIERS
1	1	1	7	7

Rev. of Const. E-2337-38.L

STA. 8+780
Req'd. Project Marker
Remove & Rebuild 200'
Board Fence

Note:
Rate of Superelevation of 15° curve
to be 0.02 ft. per foot width of roadway.

Note:
At the following Stations
Manhole Fittings and Covers
are to be adjusted to grade
of New Pavement.

- Sta. 10+125
- 14+175
- 30+190
- 49+650
- 52+530
- 55+050

STA. 13+974
Req'd. 20'x180' Concrete Box Culvert

STA. 14+328
Req'd. 20'x140' Concrete Box Culvert

STA. 18+330
Remove 15'x30' C.M.P.

STA. 17+824
Req'd. 20'x150' Conc. Box CULV

Remove 15'x30' C.M.P.

STA. 21+574
Req'd. 20'x180' Conc. Box CULV

Remove 24'x36' Wood Box

STA. 22+026
Req'd. 20'x180' Conc. Box CULV

Remove 24'x36' Wood Box

STA. 25+024
Req'd. 20'x180' Concrete Box Culvert

Remove 15'x34' C.M.P.

STA. 25+376
Req'd. 20'x180' Concrete Box Culvert

Remove 15'x34' C.M.P.

SW 4 SEC. 32

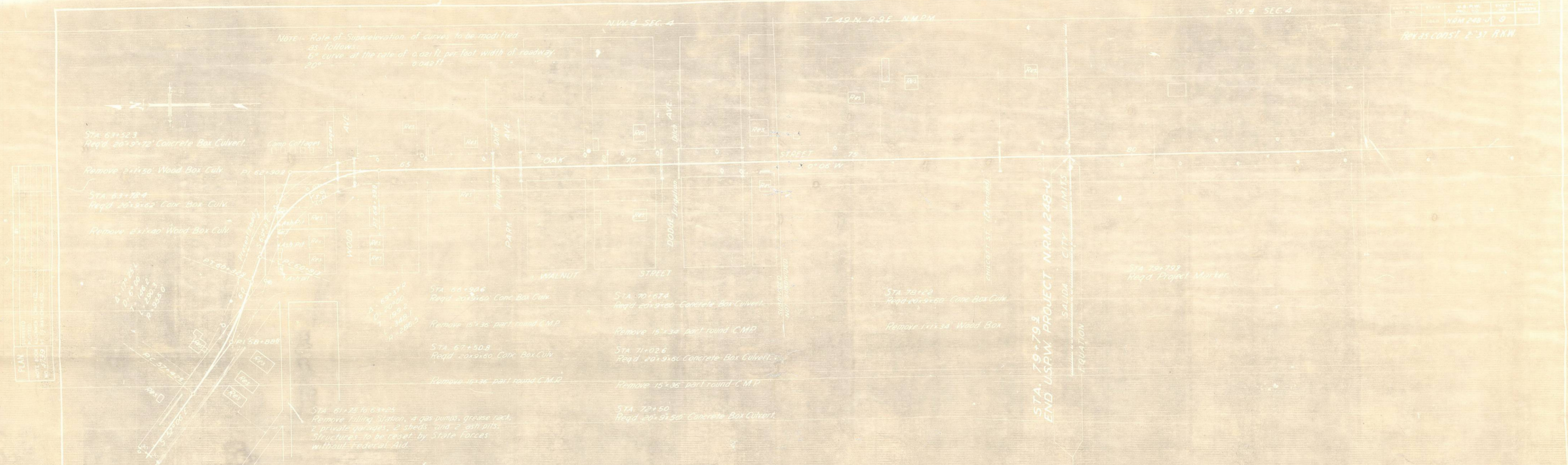


PRELIMINARY DATA

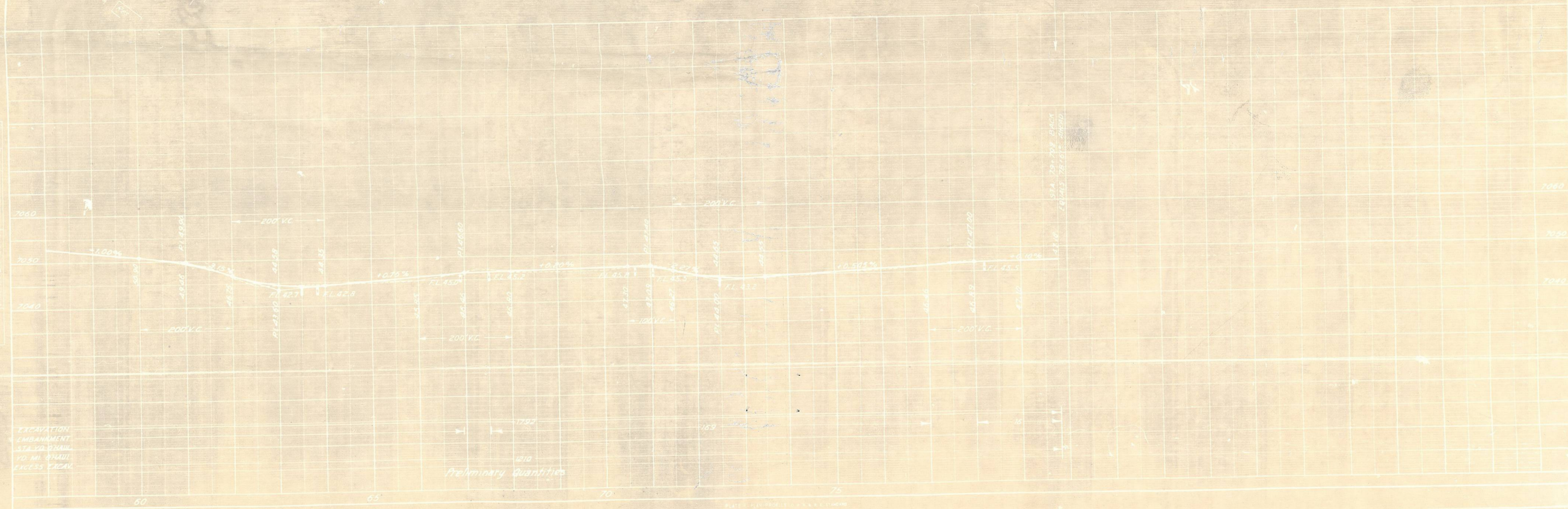
225	277	287
225	277	287
225	277	287

DATE	BY	CHECKED	DATE
10/25/50	W.M.	J.M.	10/25/50
REV. 25 CONST. 2' AT R/W			

Note: Rate of Superelevation of curves to be modified as follows:
 6" curve at the rate of 1 cent ft per foot width of roadway
 20"



PLAN	DATE	BY	CHECKED
10/25/50	10/25/50	W.M.	J.M.



EXCAVATION
 EMBANKMENT
 STA. 50+00 TO 50+100
 PD. NO. 1000
 EXCESS 1000

Preliminary Quantities

DATE: 10/25/50

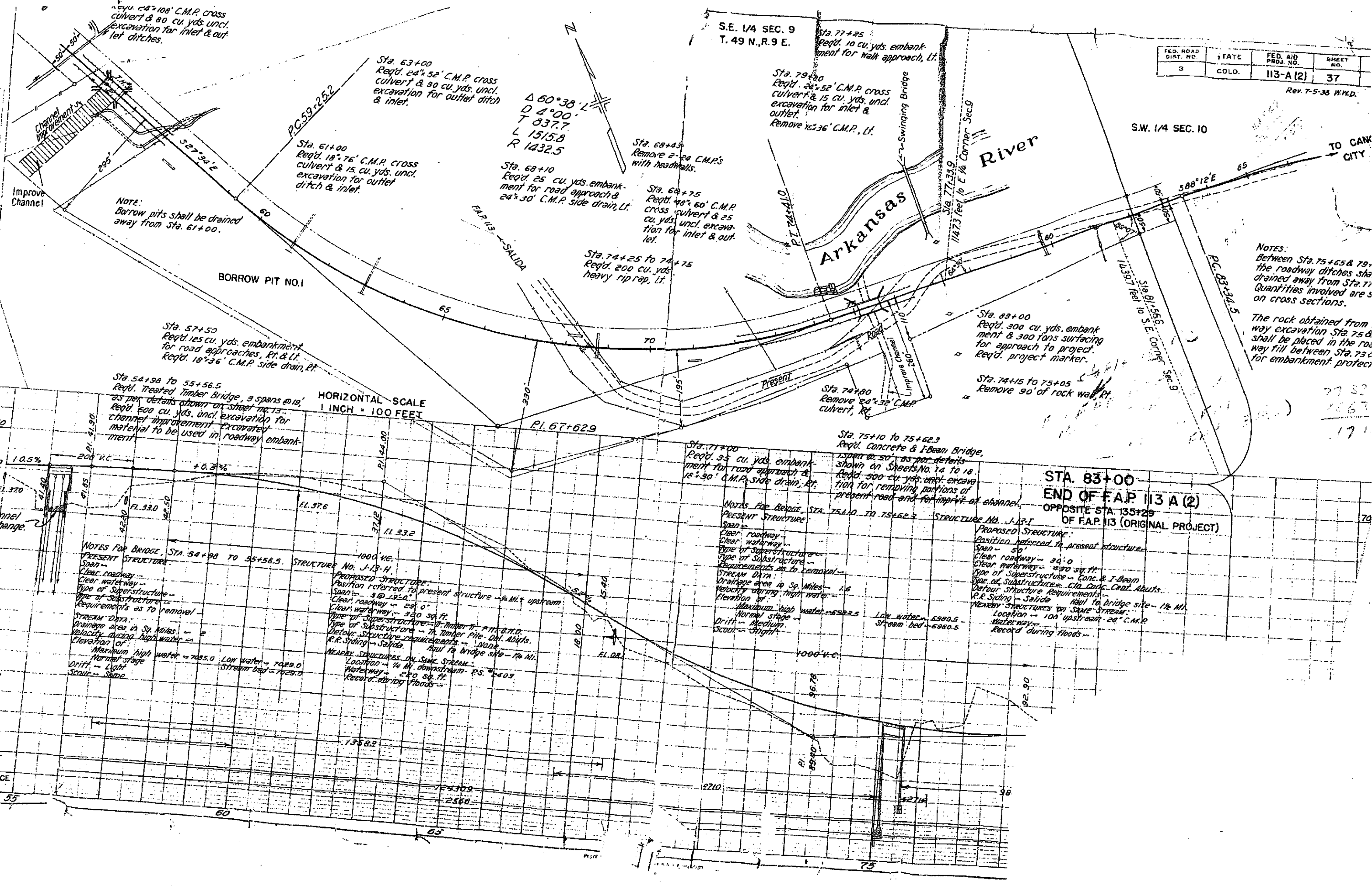
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.
3	COLO.	113-A (2)	37

Rev. 7-5-38 M.W.D.

S.E. 1/4 SEC. 9
T. 49 N., R. 9 E.

S.W. 1/4 SEC. 10

TO CANY CITY



Reqd. 24'x108' C.M.P. cross culvert & 80 cu. yds. uncl. excavation for inlet & outlet ditches.

Sta. 63+00
Reqd. 24'x52' C.M.P. cross culvert & 30 cu. yds. uncl. excavation for outlet ditch & inlet.

Sta. 61+00
Reqd. 18'x76' C.M.P. cross culvert & 15 cu. yds. uncl. excavation for outlet ditch & inlet.

$\Delta 60^{\circ}38' L$
 $D 4'00'$
 $T 837.7$
 $L 1515.8$
 $R 1432.5$

Sta. 68+45
Remove 2'x24' C.M.P.'s with headwalls.

Sta. 68+75
Reqd. 48'x60' C.M.P. cross culvert & 25 cu. yds. uncl. excavation for inlet & outlet.

Sta. 77+25
Reqd. 10 cu. yds. embankment for walk approach, Lt.

Sta. 79+00
Reqd. 24'x52' C.M.P. cross culvert & 15 cu. yds. uncl. excavation for inlet & outlet.
Remove 15'x36' C.M.P., Lt.

Sta. 68+10
Reqd. 25 cu. yds. embankment for road approach & 24'x30' C.M.P. side drain, Lt.

Sta. 74+25 to 74+75
Reqd. 200 cu. yds. heavy rip rap, Lt.

Sta. 74+00
Remove 24'x36' C.M.P. culvert, Rt.

Sta. 83+00
Reqd. 300 cu. yds. embankment & 300 tons surfacing for approach to project.
Reqd. project marker.

Sta. 74+15 to 75+05
Remove 90' of rock wall, Rt.

NOTES:
Between Sta. 75+65 & 79+ the roadway ditches shall be drained away from Sta. 77. Quantities involved are as on cross sections.
The rock obtained from way excavation Sta. 75 & shall be placed in the roadway fill between Sta. 73 & for embankment protecti

NOTE:
Borrow pits shall be drained away from Sta. 61+00.

Sta. 57+50
Reqd. 125 cu. yds. embankment for road approaches, Pt. & Lt.
Reqd. 18'x36' C.M.P. side drain, Pt.

Sta. 54+98 to 55+56.5
Reqd. Treated Timber Bridge, 3 spans @ 19', as per details shown on sheet No. 13.
Reqd. 500 cu. yds. uncl. excavation for channel improvement. Excavated material to be used in roadway embankment.

HORIZONTAL SCALE
1 INCH = 100 FEET

Sta. 71+00
Reqd. 35 cu. yds. embankment for road approach & 15'x30' C.M.P. side drain, Rt.

Sta. 75+10 to 75+62.3
Reqd. Concrete & I-Beam Bridge, 1 span @ 50', as per details shown on Sheets No. 14 to 18.
Reqd. 500 cu. yds. uncl. excavation for removing portions of present road and for improvt of channel.

STA. 83+00
END OF F.A.P. 113 A (2)
OPPOSITE STA. 135+29
OF F.A.P. 113 (ORIGINAL PROJECT)

NOTES FOR BRIDGE, STA. 54+98 TO 55+56.5. STRUCTURE No. J-13-H.
PRESENT STRUCTURE:
Span - 1000' V.C.
Clear roadway - 200'
Clear waterway - 200'
Type of Superstructure -
Type of Substructure -
Requirements as to removal -
STREAM DATA:
Drainage area in Sq. Miles -
Velocity during high water -
Elevation of -
Maximum high water - 7035.0
Normal stage -
Drift - Light
Scour - Same

PROPOSED STRUCTURE:
Position referred to present structure - 1/4 Mi. upstream
Span - 3 @ 19'-0"
Clear roadway - 200'
Clear waterway - 320 sq. ft.
Type of Superstructure - Timber Tr. Pile & D.T.D.
Type of Substructure - Timber Pile, Dnt. Abutts.
Detour Structure requirements - None
R.R. Siding - Salida
NEARBY STRUCTURES ON SAME STREAM:
Location - 1/4 Mi. downstream - P.S. 2403
Waterway - 220 sq. ft.
Record during floods -

NOTES FOR BRIDGE, STA. 75+10 TO 75+62.3. STRUCTURE No. J-13-I.
PRESENT STRUCTURE:
Span -
Clear roadway -
Clear waterway -
Type of Superstructure -
Type of Substructure -
Requirements as to removal -
STREAM DATA:
Drainage area in Sq. Miles - 1.6
Velocity during high water -
Elevation of -
Maximum high water - 6923.5
Normal stage -
Drift - Medium
Scour - Slight

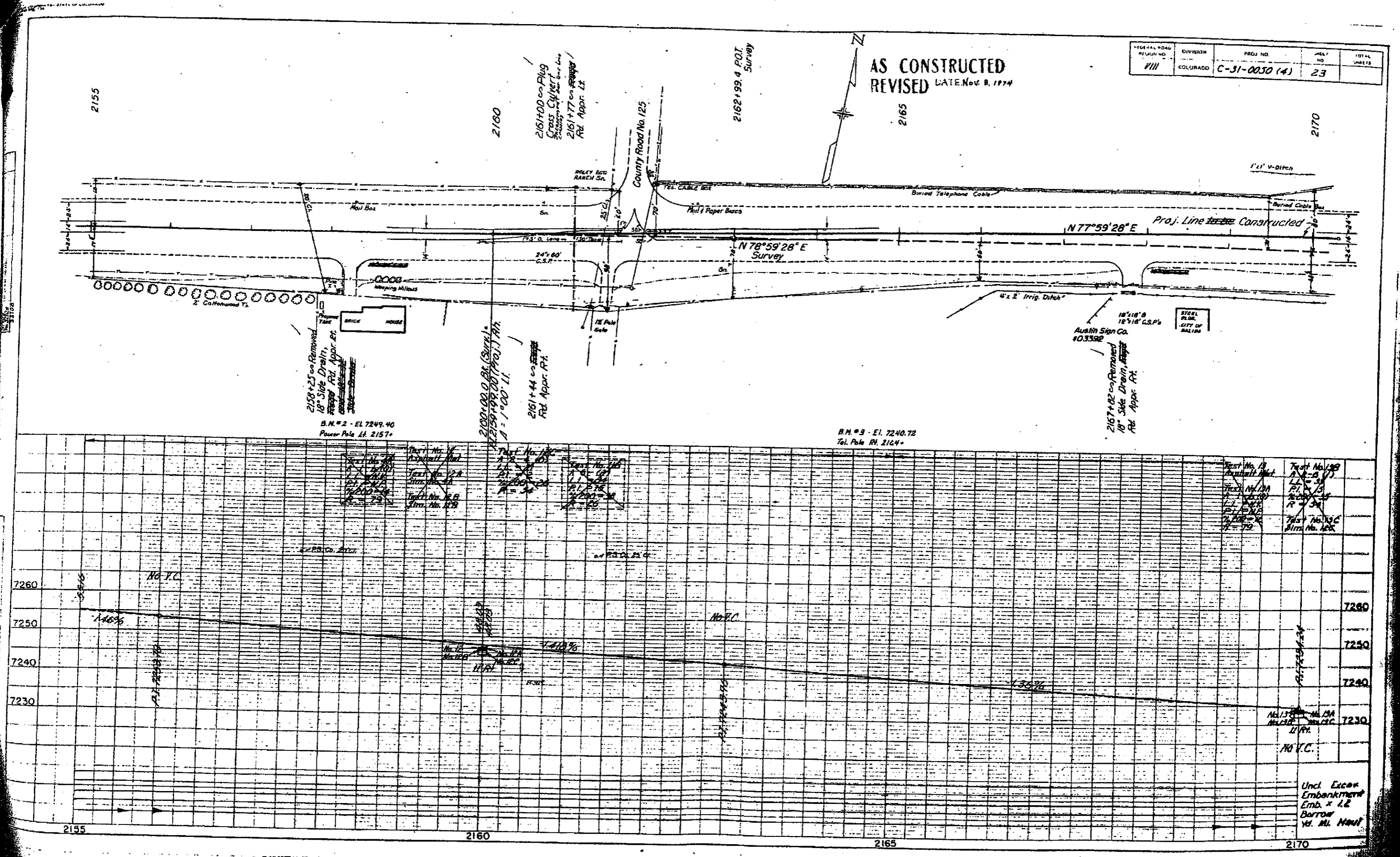
PROPOSED STRUCTURE:
Position referred to present structure -
Span - 50'
Clear roadway - 30'
Clear waterway - 320 sq. ft.
Type of Superstructure - Conc. & I-Beam
Type of Substructure - Cila, Conc. Cant. Abutts.
Detour Structure Requirements -
R.R. Siding - Salida
Location - 1/4 Mi. upstream - 24' C.M.P.
Waterway -
Record during floods -

CAV
R+15%
TROW
T.O. HAUL
M.O. HAUL
ROW SOURCE

77.82
1867
17

GENERAL DRAW REVISION NO.	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
VIII	COLORADO	C-31-0050 (4)	23	

AS CONSTRUCTED
REVISED LATE NOV. 8, 1974



B.M. #2 - EL 7249.40
Power Pole Lt. 2157+

B.M. #3 - EL 7240.72
Tel. Pole Rt. 2164+

Test No. 12 No. 12 No. 12 No. 12 No. 12 No. 12 No. 12 No. 12	Test No. 13 No. 13 No. 13 No. 13 No. 13 No. 13 No. 13 No. 13	Test No. 14 No. 14 No. 14 No. 14 No. 14 No. 14 No. 14 No. 14	Test No. 15 No. 15 No. 15 No. 15 No. 15 No. 15 No. 15 No. 15	Test No. 16 No. 16 No. 16 No. 16 No. 16 No. 16 No. 16 No. 16
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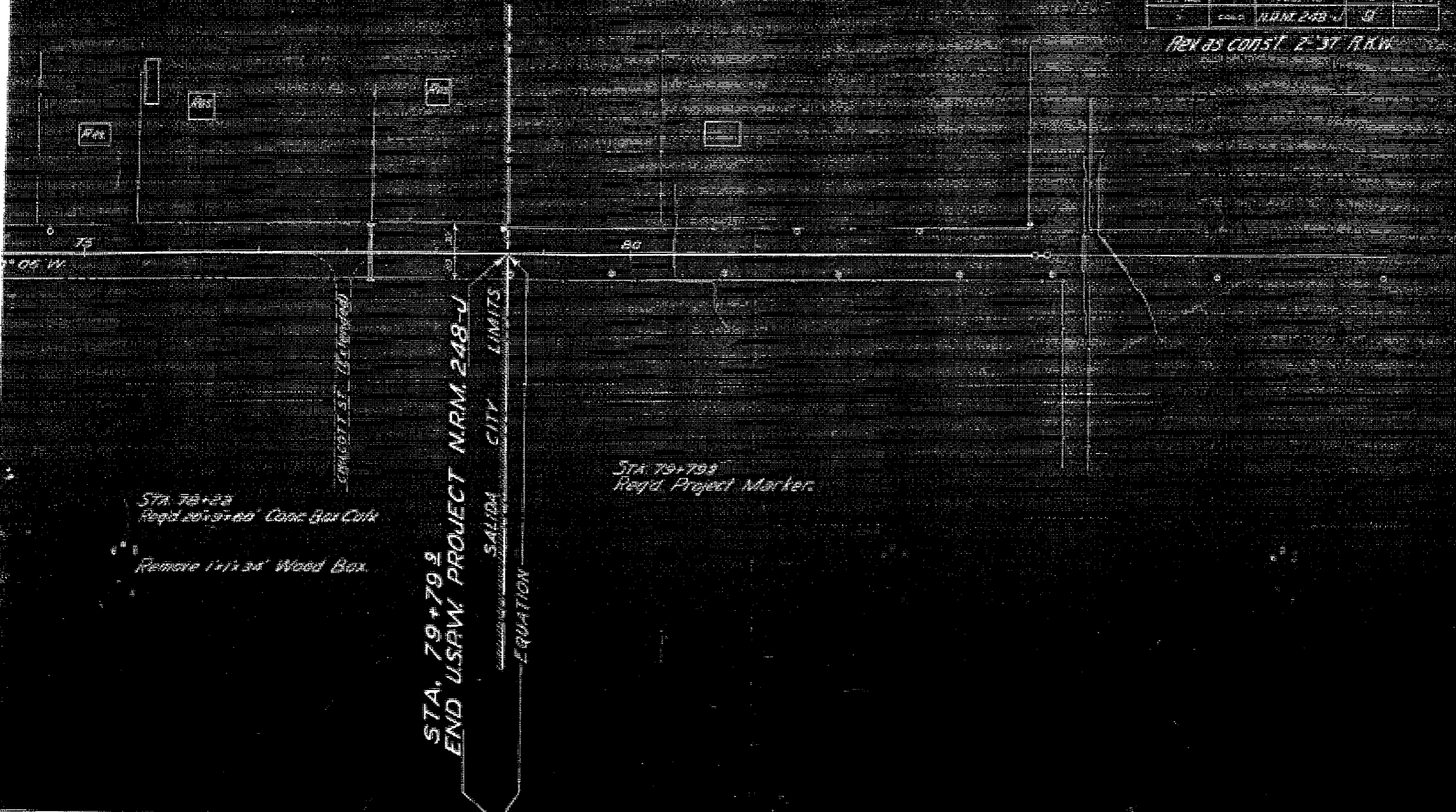
Uncl. Excor.
Embankment
Emb. x 1.2
Borrow
Vol. M. Haul

T 49 N R 3E N.M.P.M.

SW 4 SEC 4

FEED	STAKE	U.S. PROJ.	DEPTH	TOTAL
INSTR.	NO.	NO.	NO.	DEPTH
	1002	N.M.P.M. 248-U	5'	

REV AS CONST 2-37 R.K.W.



STA. 79+23
Road 2013+20 Conc. Box Coll

Remove 1-17-36 Wood Box

STA. 79+79 1/2
END USAW PROJECT NRM 248-U

SALIDA CITY LIMITS
EQUATION

STA. 79+79 1/2
Road Project Marker

N.W. 4 SEC. 32

STA. 8+79.0
Req'd Project Marker.
Remove & Rebuild 200'
Board fence.

STA. 5+128 Ahead
EQUALS 5+198 Back
P.I. 4+275 Δ=0°26' 17.90"

Δ=49°27'R
D=15°00'
T=175.9'
L=329.7'
R=382.0'

Note:
Rate of Superelevation of 15%
to be 0.021 ft. per foot width

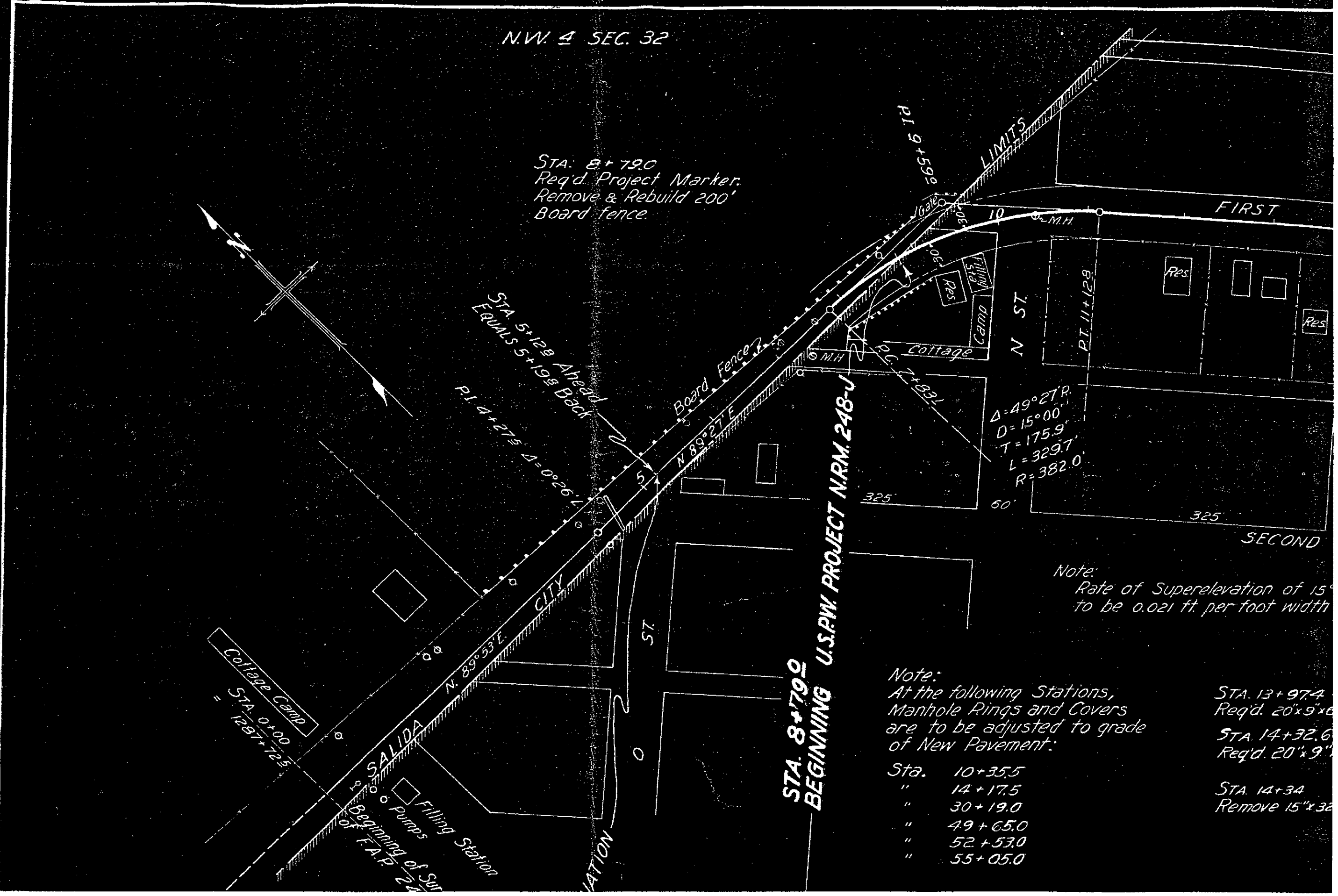
Note:
At the following Stations,
Manhole Rings and Covers
are to be adjusted to grade
of New Pavement:

- Sta. 10+35.5
- " 14+17.5
- " 30+19.0
- " 49+65.0
- " 52+53.0
- " 55+05.0

- STA. 13+97.4
Req'd. 20"x9"x6"
- STA. 14+32.6
Req'd. 20"x9"
- STA. 14+34
Remove 15"x32"

STA. 8+79.0
BEGINNING U.S.P.W. PROJECT N.R.M. 248-J

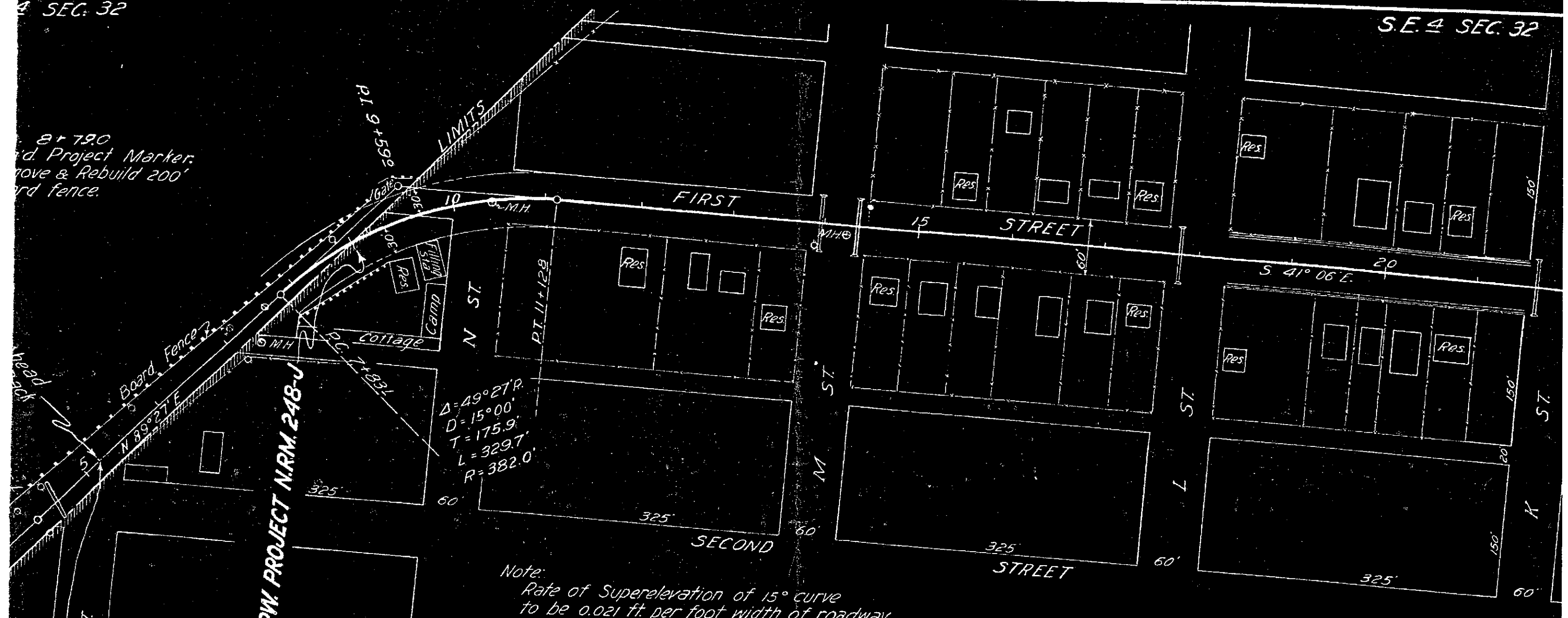
PLAN	SURVEYED	DATE
	PLOTTED	
NOTE BOOK NO. 4739	ALIGNMENT CHECKED	
	RT. OF WAY CHECKED	



SEC. 32

S.E. 4 SEC. 32

8+790
d Project Marker.
rove & Rebuild 200'
ard fence.



STA. 8+790
BEGINNING U.S.P.W. PROJECT N.R.M. 248-J

Note:
At the following Stations,
Manhole Rings and Covers
are to be adjusted to grade
of New Pavement:

- Sta. 10+35.5
- " 14+17.5
- " 30+19.0
- " 49+65.0
- " 52+53.0
- " 55+05.0

STA. 13+974
Req'd. 20'x9'x60' Concrete Box Culvert.

STA. 14+32.6
Req'd. 20'x9'x60' Concrete Box Culvert.

STA. 14+34
Remove 15'x32' C.M.P.

STA. 17+824
Req'd. 20'x9'x60' Conc. Box Culv.

Remove 15'x32' C.M.P.

STA. 21+674
Req'd. 20'x9'x60'

Remove 2'x1'x3'

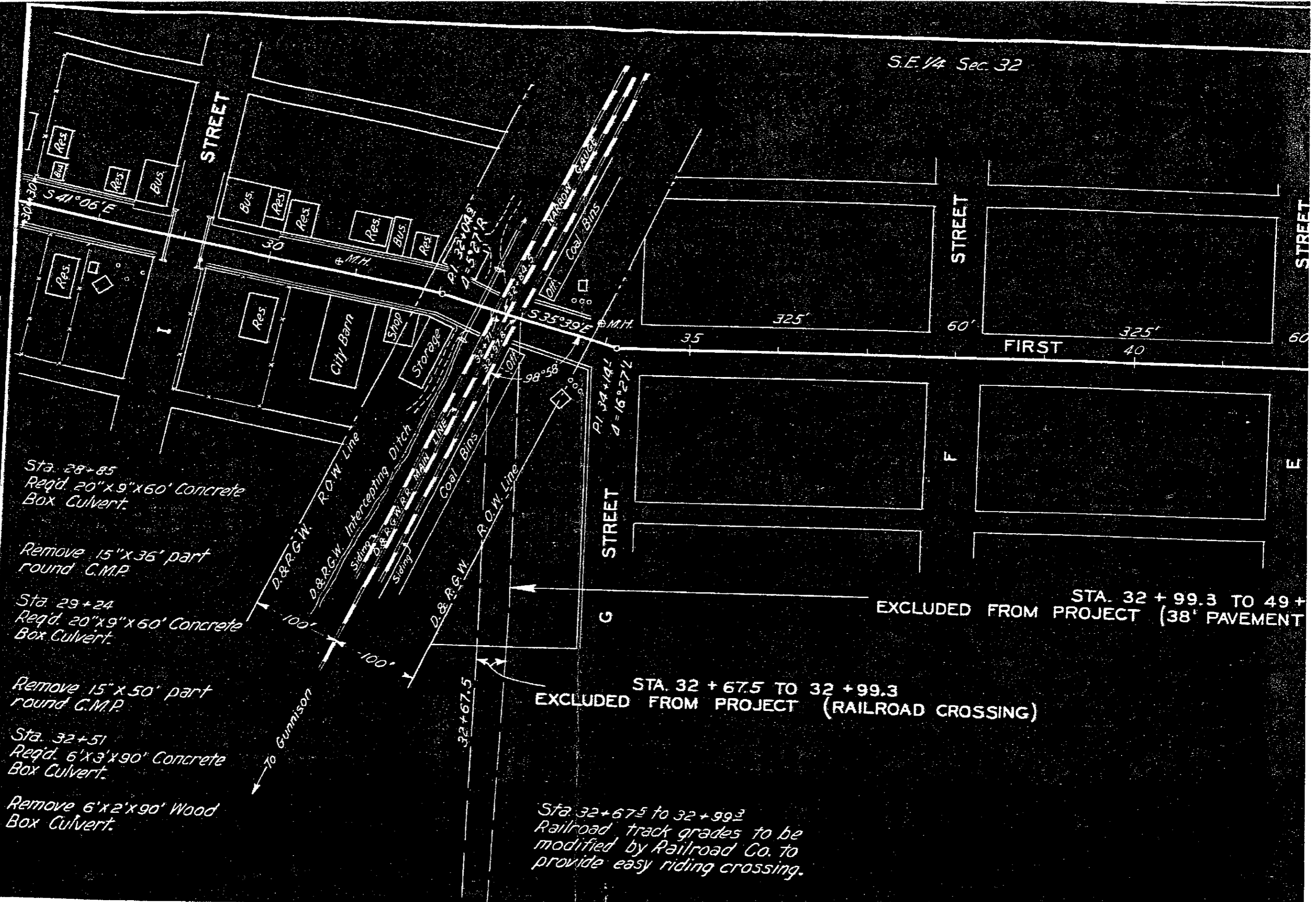
STA. 22+02.0
Req'd. 20'x9'x60'

Remove 2'x1'x3'

QUATION

S.E. 1/4 Sec. 32

PLAN	SURVEYED	BY	DATE
	NOTE BOOK NO. 4739		
	PLOTTED		
	ALIGNMENT CHECKED		
	PT. OF WAY CHECKED		



Sta. 28+85
Req'd. 20"x9"x60' Concrete
Box Culvert.

Remove 15"x36" part
round C.M.P.

Sta. 29+24
Req'd. 20"x9"x60' Concrete
Box Culvert.

Remove 15"x50" part
round C.M.P.

Sta. 32+51
Req'd. 6'x3'x90' Concrete
Box Culvert.

Remove 6'x2'x90' Wood
Box Culvert.

STA. 32+67.5 TO 32+99.3
EXCLUDED FROM PROJECT (RAILROAD CROSSING)

STA. 32+99.3 TO 49+
EXCLUDED FROM PROJECT (38' PAVEMENT)

Sta. 32+67.5 to 32+99.3
Railroad track grades to be
modified by Railroad Co. to
provide easy riding crossing.

S.E. 4 SEC. 32 T. 50N. R. 9E. N.M.P.M.

FED ROAD DIST NO.	STATE	U.S.P.W. PROJ NO.	SHEET NO.	TOTAL SHEETS
3	COLD	N.R.M. 248-U	7	

Rev as Const. 2-23-37-S.B.L.



STA. 17+82.4
 Req'd. 20'x9'x60' Conc. Box Culv.
 Remove 15'x32' C.M.P.

STA. 21+67.4
 Req'd. 20'x9'x60' Conc. Box Culv.
 Remove 2'x1'x36' Wood Box.

STA. 25+02.4
 Req'd. 20'x9'x60' Concrete Box Culvert.
 Remove 15'x34' C.M.P.

STA. 22+02.6
 Req'd. 20'x9'x60' Conc. Box Culv.
 Remove 2'x1'x36' Wood Box.

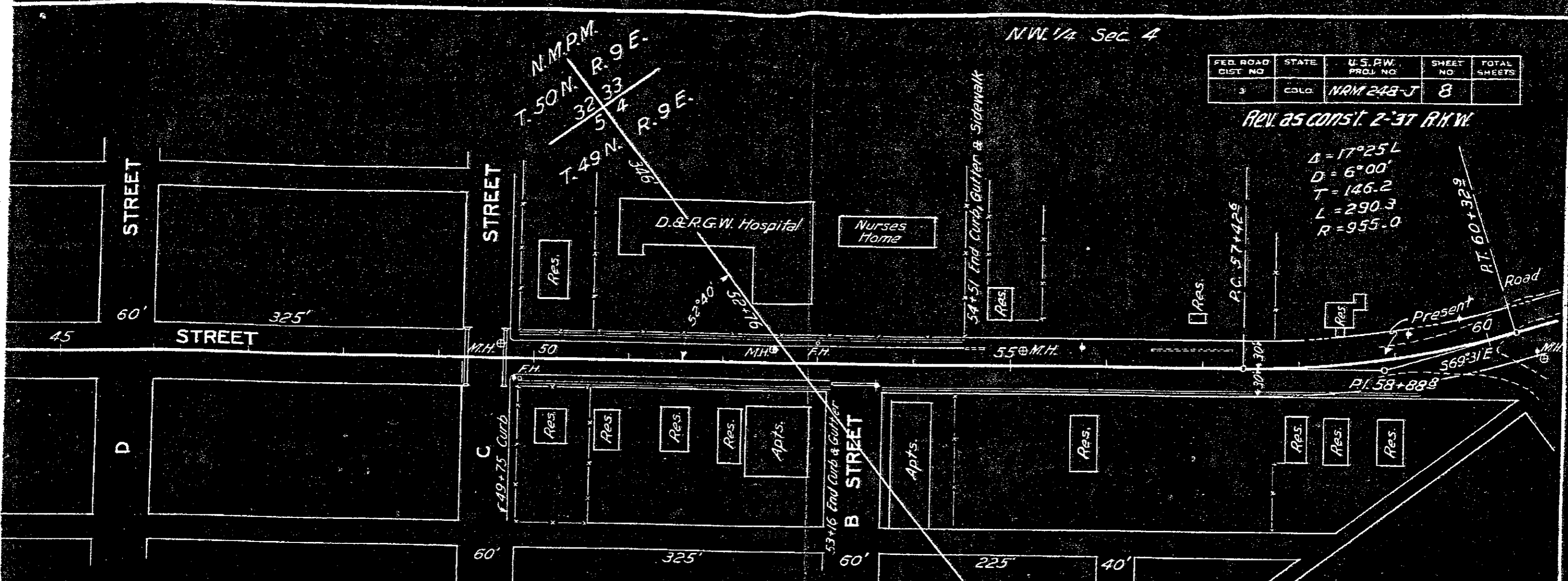
STA. 25+37.6
 Req'd. 20'x9'x60' Concrete Box Culvert.

N.W. 1/4 Sec 4

FED. ROAD DIST NO	STATE	U.S.P.W. PROJ NO	SHEET NO	TOTAL SHEETS
3	COLO.	NRM 248-J	8	

REV. AS CONST. 2-31 R.H.W.

$\Delta = 17^{\circ}25' L$
 $D = 6^{\circ}00'$
 $T = 146.2$
 $L = 290.3$
 $R = 955.0$



Sta. 49+31 to 49+79
 Remove 29795 Sq. Yds.
 Rock Asphalt Pavement.

Sta. 53+40
 Req'd. 18" x 50' C.M.P. side
 drain-Right.
 Remove 15" x 1' x 38' Wood
 Box Culvert.

Sta. 49+37.4
 Req'd. 20" x 6" x 60' Concrete
 Box Culvert.

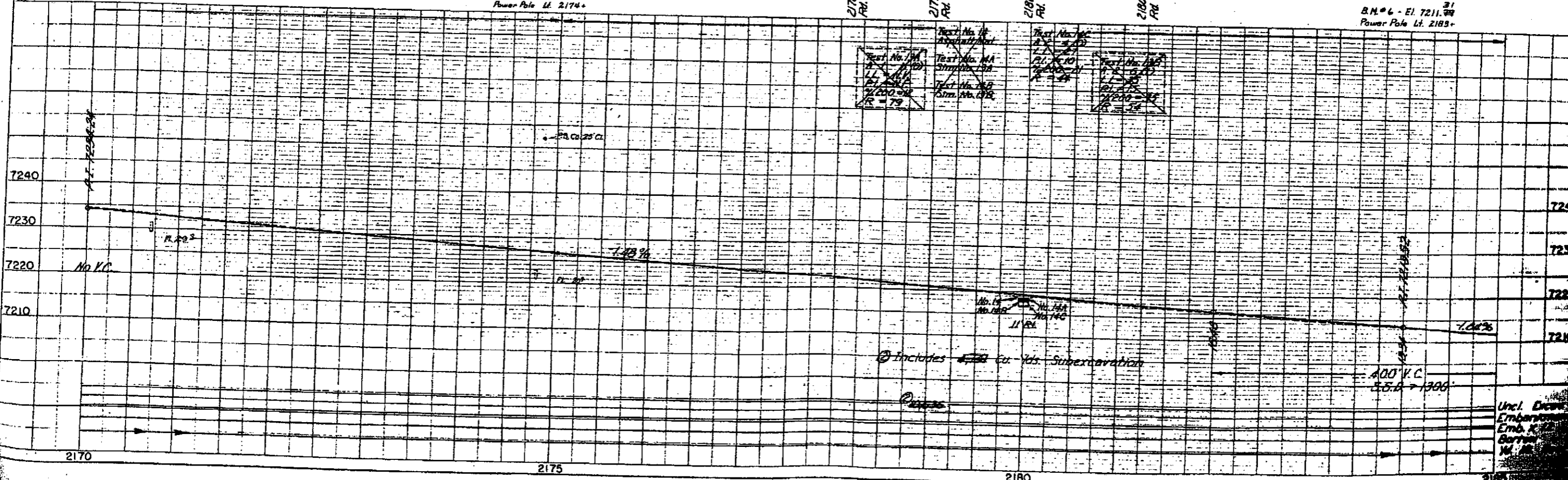
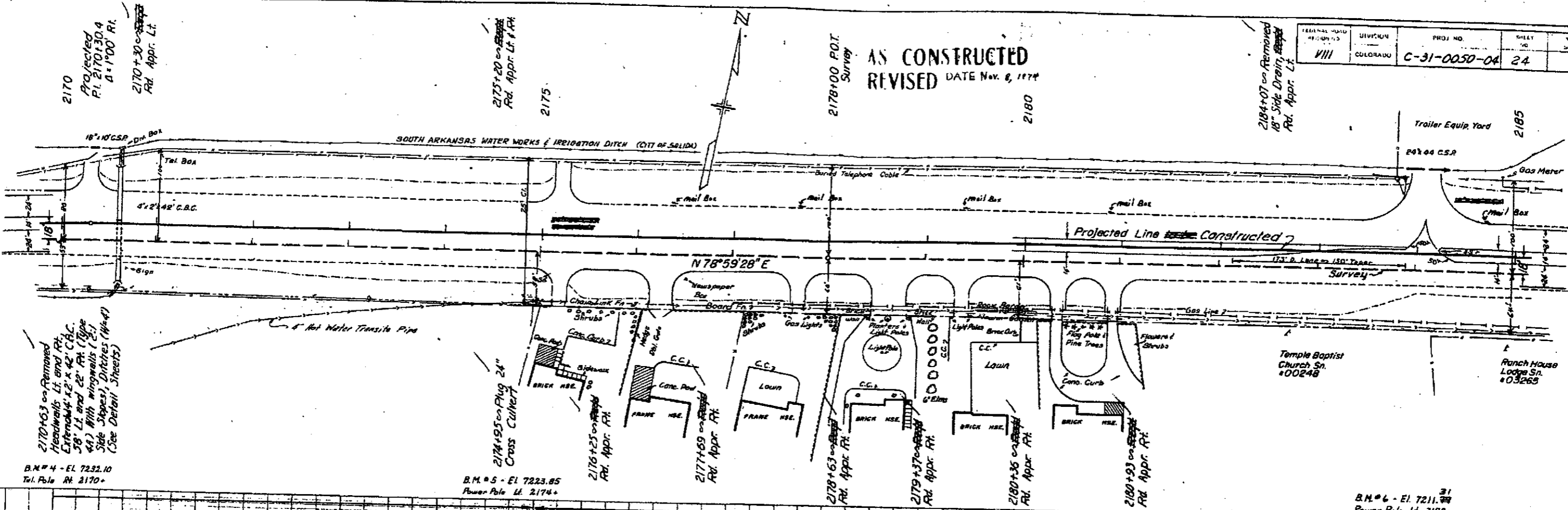
Sta. 54+60
 Remove 1' x 1' x 14' Wood
 Box Side Drain-Left.

Sta. 57+00
 Remove 1' x 1' x 90' Wood
 Box Side Drain-Left.

Sta. 49+72.6
 Req'd. 20" x 6" x 60' Concrete
 Box Culvert.

FEDERAL ROAD DISTRICT NO. VIII	DIVISION COLORADO	PROJ. NO. C-31-0050-04	SHEET NO. 24	TOTAL SHEETS
--------------------------------	-------------------	------------------------	--------------	--------------

AS CONSTRUCTED
REVISED DATE Nov. 8, 1974

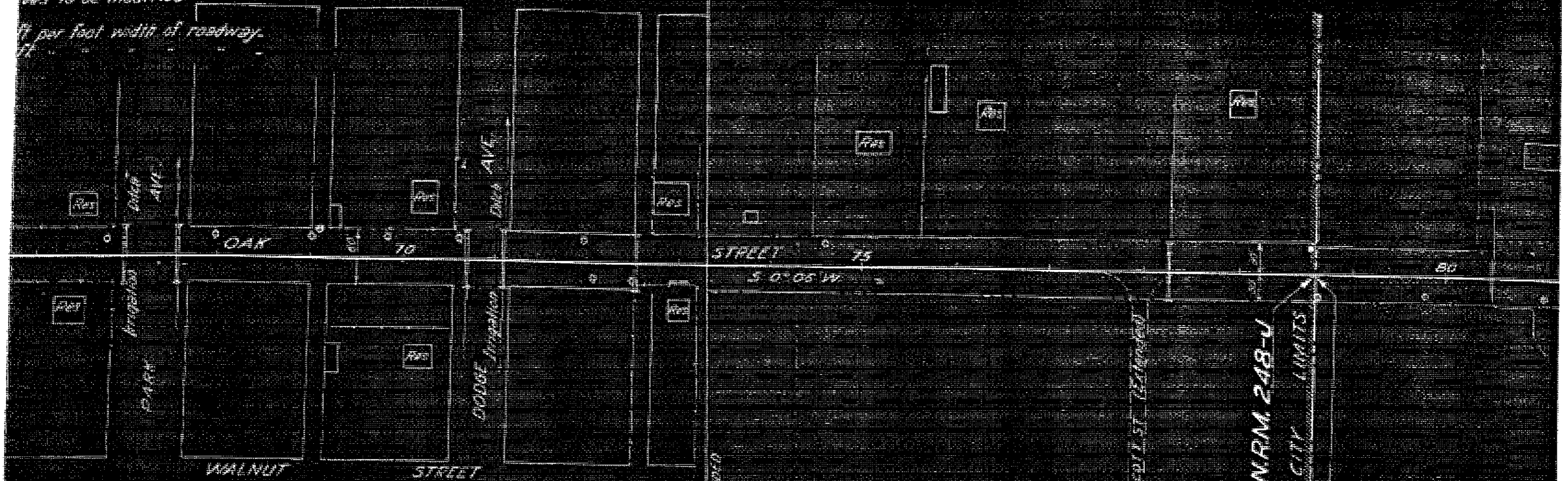


Uncl. Excav.
Emb.
Borrow
M. M.

lines to be modified
1" per foot width of roadway.

N.W. 1/4 SEC. 4

T. 49 N. R. 9 E. N.M.P.M.



Sta. 55+52.6
Reqd. 20'x9'x60' Conc. Box Culvert

Remove 15'x36" part round CMP

Sta. 57+50.8
Reqd. 20'x9'x60' Conc. Box Culvert

Remove 15'x36" part round CMP

Inst. 4" x 4" oak
4" x 2" oak poles
to forces

STA. 70+67.4
Reqd. 20'x9'x60' Concrete Box Culvert

Remove 15'x36" part round CMP

STA. 71+02.6
Reqd. 20'x9'x60' Concrete Box Culvert

Remove 15'x36" part round CMP

STA. 72+50
Reqd. 20'x9'x30' Concrete Box Culvert

STA. 78+28
Reqd. 20'x9'x60' Conc. Box Culvert

Remove 15'x36" Wood Box

STA. 79+79.9
END USRW PROJECT NRM 248-U

SALIDA CITY LIMITS

EQUATION

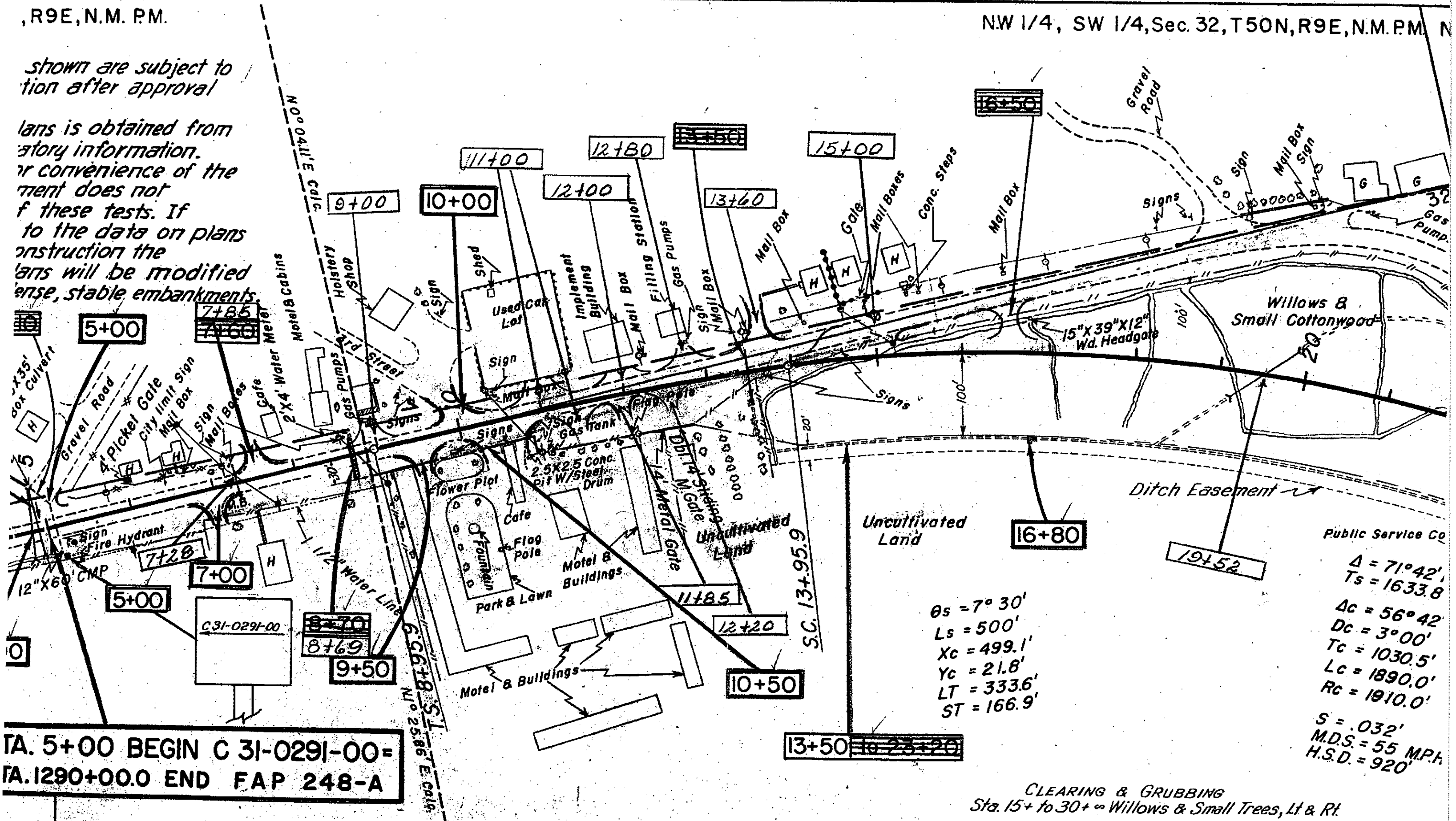
STA. 79+79.9
Reqd. Project

, R9E, N.M. P.M.

NW 1/4, SW 1/4, Sec. 32, T50N, R9E, N.M. P.M. N

shown are subject to
tion after approval

Plans is obtained from
atory information.
r convenience of the
ment does not
f these tests. If
to the data on plans
nstruction the
ans will be modified
ense, stable embankments.



TA. 5+00 BEGIN C 31-0291-00=
 TA. 1290+00.0 END FAP 248-A

$\theta_s = 7^\circ 30'$
 $L_s = 500'$
 $X_c = 499.1'$
 $Y_c = 21.8'$
 $LT = 333.6'$
 $ST = 166.9'$

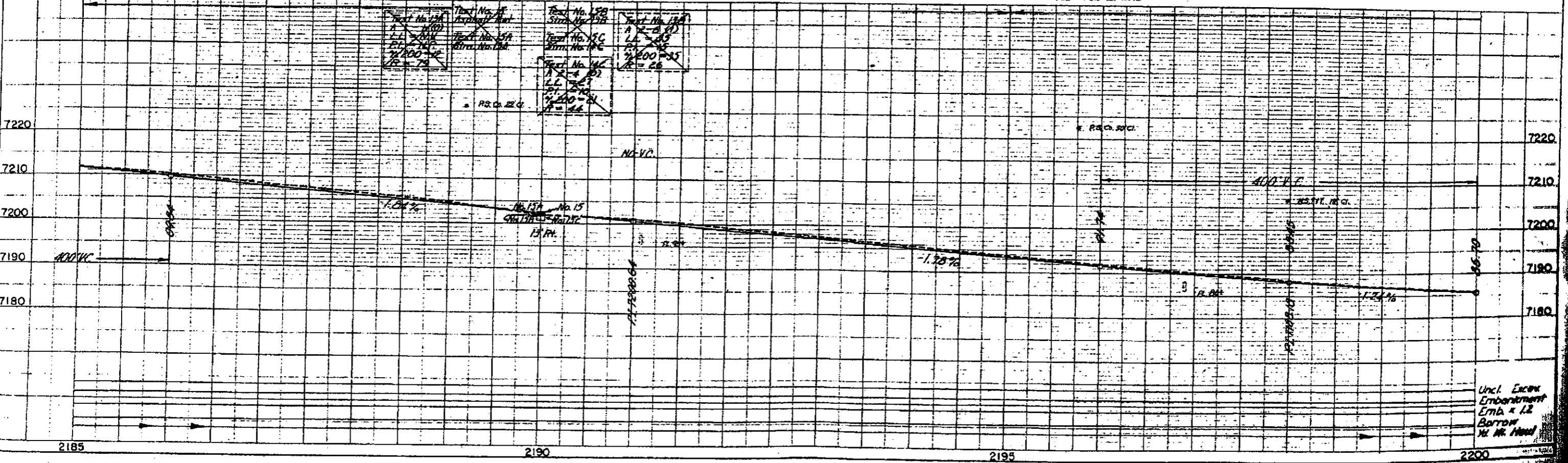
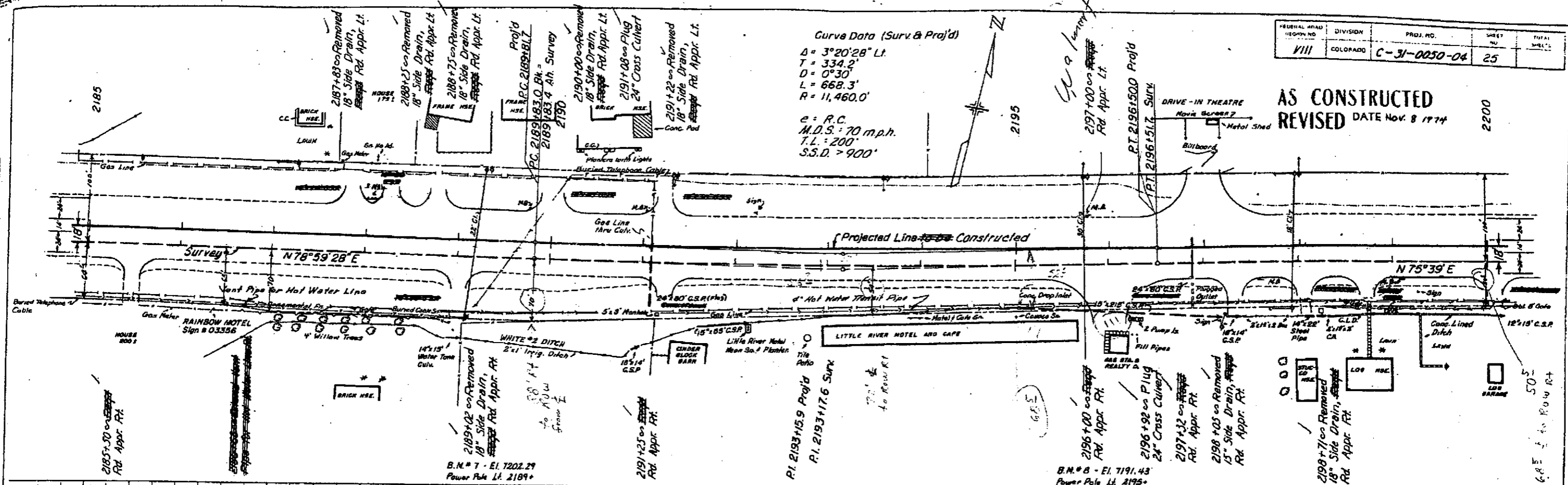
Public Service Co
 $\Delta = 71^\circ 42'$
 $T_s = 1633.8$
 $\Delta c = 56^\circ 42'$
 $D_c = 3^\circ 00'$
 $T_c = 1030.5'$
 $L_c = 1890.0'$
 $R_c = 1910.0'$
 $S = .032'$
 $M.D.S. = 55 \text{ MPH}$
 $H.S.D. = 920'$

CLEARING & GRUBBING
 Sta. 15+ to 30+ Willows & Small Trees, Lt & Rt

FEDERAL ROAD DISTRICT NO.	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
VIII	COLORADO	C-31-0050-04	25	

AS CONSTRUCTED
REVISED DATE Nov. 8 1974

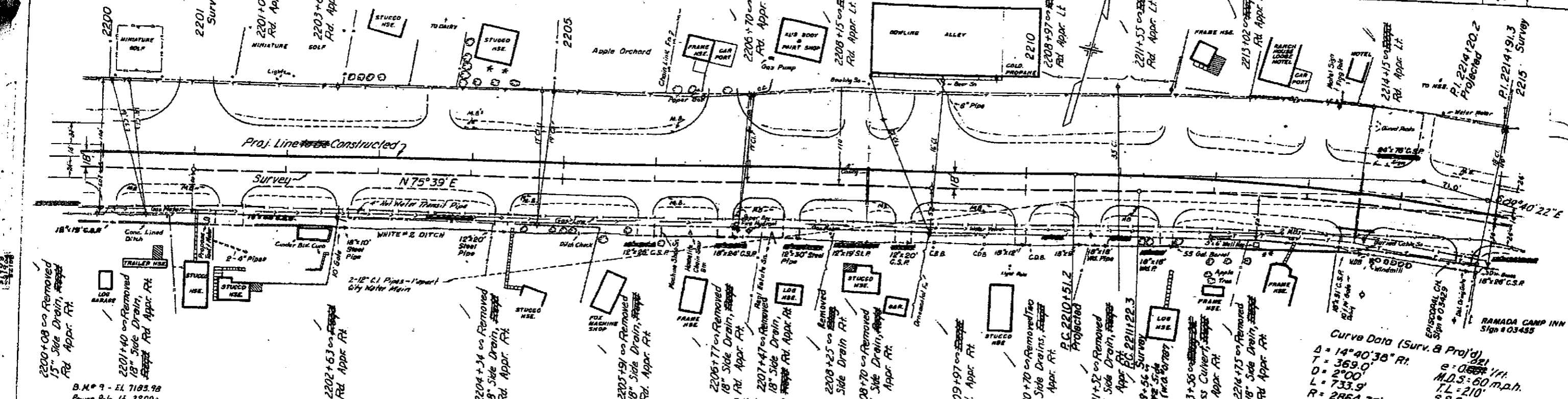
Curve Data (Surv. & Proj'd)
 $\Delta = 3^{\circ}20'28''$ Lt.
 $T = 334.2'$
 $D = 0^{\circ}30'$
 $L = 668.3'$
 $R = 11,460.0'$
 $e = R.C.$
 $M.D.S. = 70$ m.p.h.
 $T.L. = 200'$
 $S.S.D. > 900'$



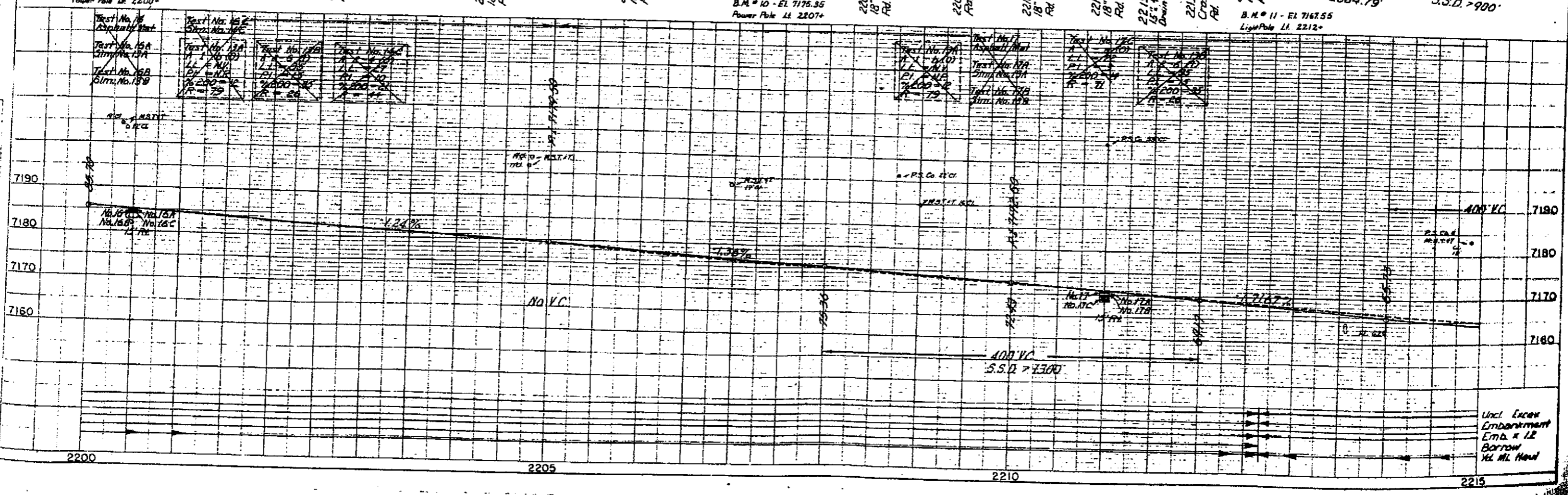
Uncl. Excav.
Embankment
Emb. x 1.2
Borrow
M. M. Road

AS CONSTRUCTED
REVISED DATE Nov. 9, 1974

SECTIONAL ROAD SECTION NO. VIII	DIVISION COLORADO	PROJ. NO. C-51-0050-04	SHEET NO. 26	TOTAL SHEETS 24615
---------------------------------------	----------------------	---------------------------	--------------------	--------------------------



Curve Data (Surv. & Proj'd)
 $\Delta = 14^{\circ}40'38''$ R.I.
 $T = 369.0$
 $D = 2^{\circ}00'$
 $L = 733.9'$
 $R = 2864.79'$
 $e = 0.066'$
 $M.S. = 60$ m.p.h.
 $T.L. = 210'$
 $S.S.D. > 900'$

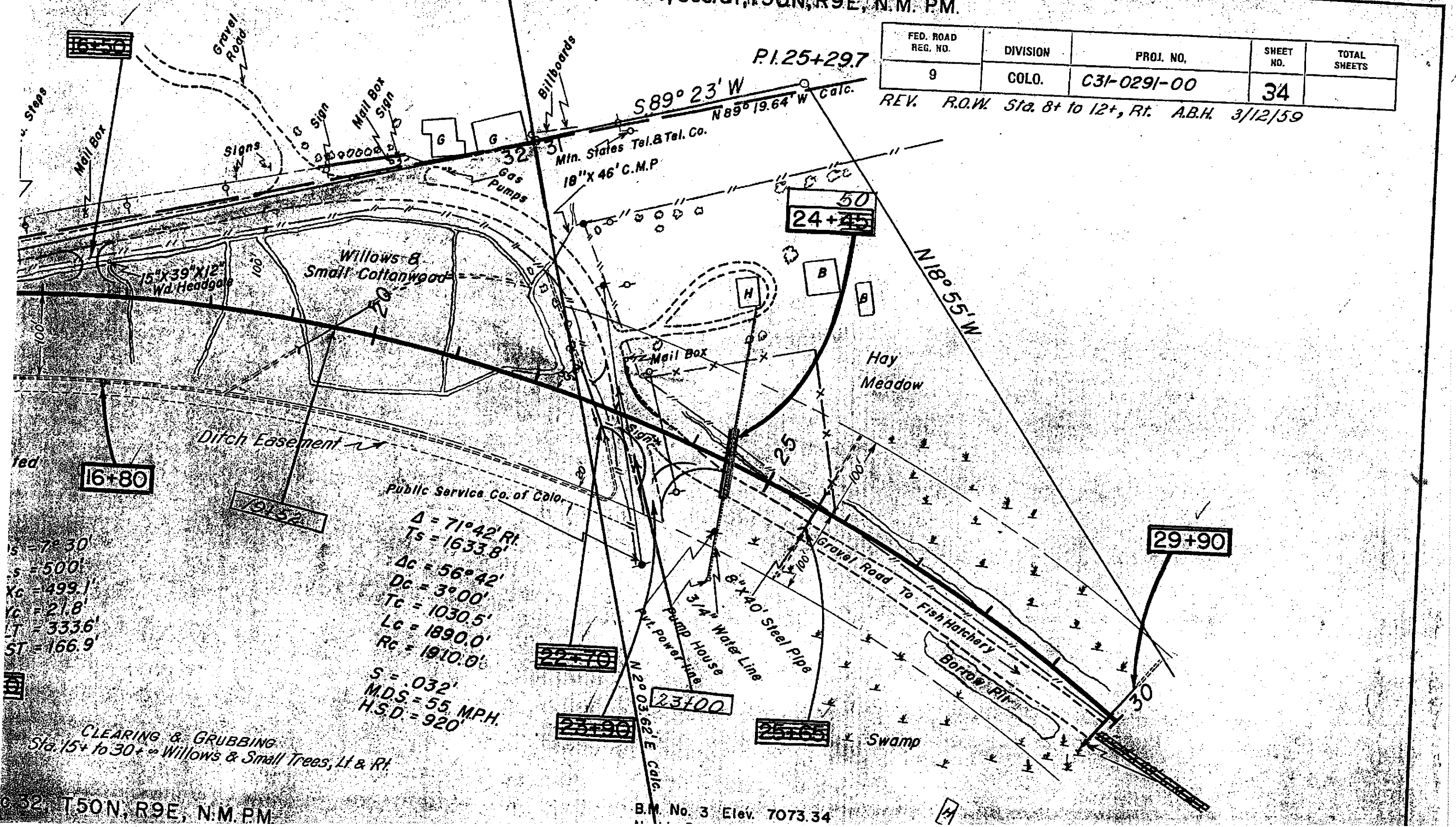


2200 2205 2210 2215

NW 1/4, SW 1/4, Sec. 32, T50N, R9E, N.M.P.M. NE 1/4, SE 1/4, Sec. 31, T50N, R9E, N.M. P.M.

FED. ROAD REG. NO.	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
9	COLO.	C31-0291-00	34	

REV. R.O.W. Sta. 8+ to 12+, Rt. A.B.H. 3/12/59



$\Delta = 7^\circ 30'$
 $S = 500'$
 $X_G = 499.1'$
 $Y_G = 21.8'$
 $L_T = 333.6'$
 $S_T = 166.9'$

$\Delta = 71^\circ 42' \text{ Rt}$
 $T_s = 1633.8'$
 $D_c = 56^\circ 42'$
 $D_c = 3^\circ 00'$
 $T_c = 1030.5'$
 $L_c = 1890.0'$
 $R_c = 1910.0'$
 $S = .032'$
 $M.D.S. = 55 \text{ MPH}$
 $H.S.D. = 920'$

CLEARING & GRUBBING
Sta. 15+ to 30+ Willows & Small Trees, Lt & Rt

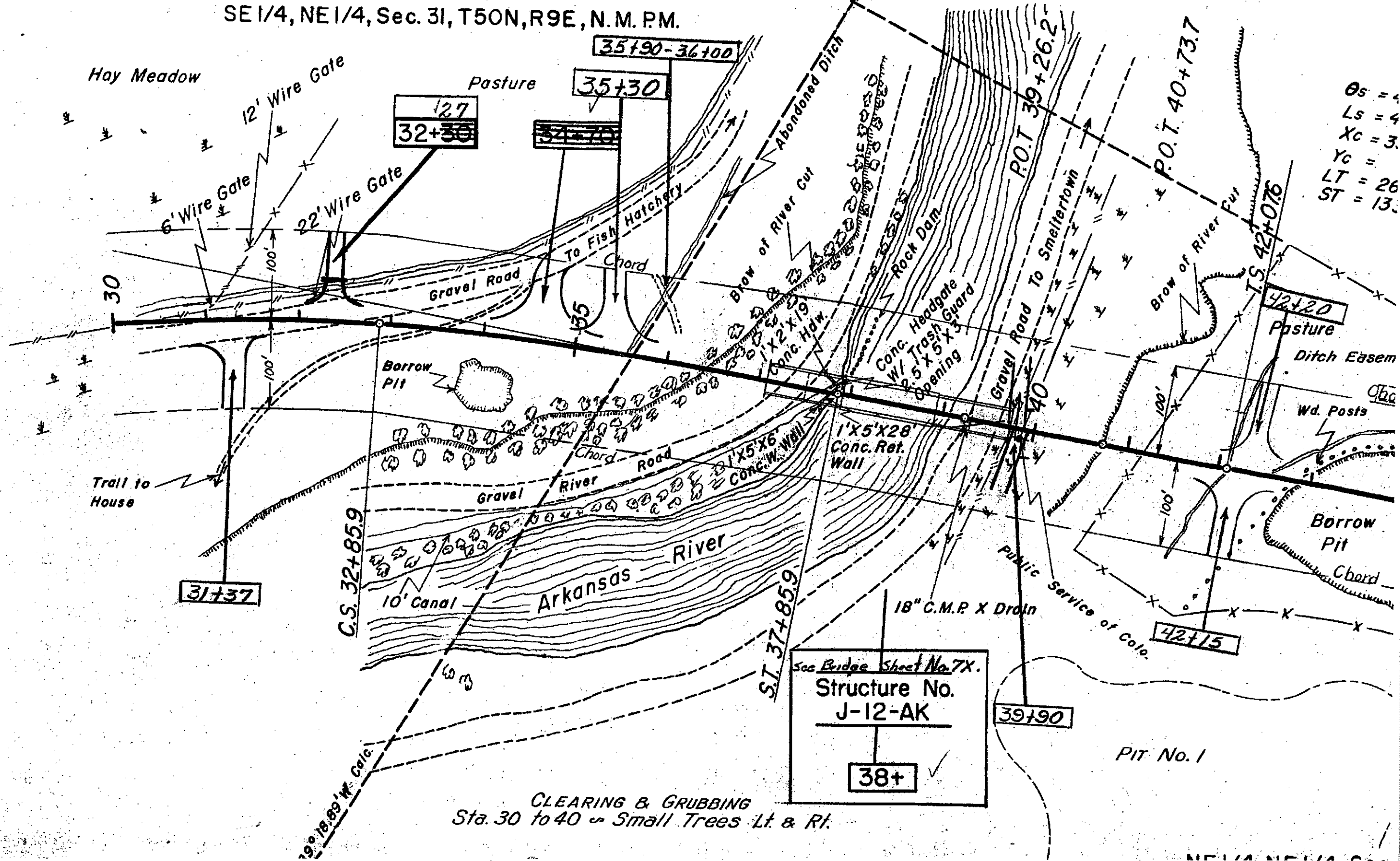
Sec. 32, T50N, R9E, N.M.P.M.

B.M. No. 3 Elev. 7073.34

SE 1/4, NE 1/4, Sec. 31, T50N, R9E, N.M.P.M.

$\theta_s = 4$
 $L_s = 4$
 $X_c = 3$
 $Y_c =$
 $LT = 26$
 $ST = 13$

PLAN	NOTE BOOK NO. 21058	SURVEYED	DATE
		PLOTTED	
		ALIGNMENT CHECKED	
		RT. OF WAY CHECKED	



See Bridge Sheet No. 7X.
Structure No. J-12-AK
38+ ✓

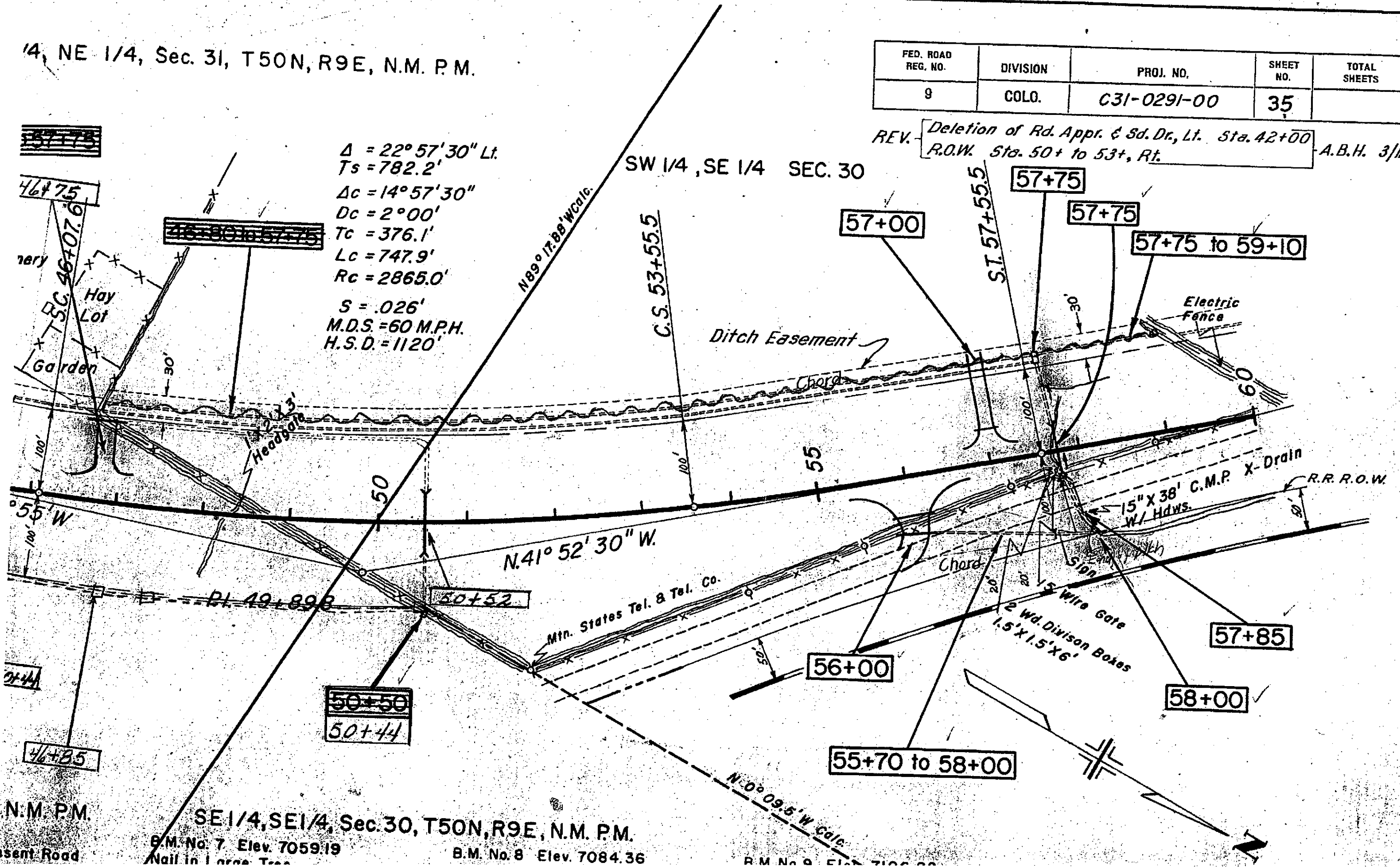
CLEARING & GRUBBING
 Sta. 30 to 40 - Small Trees Lt. & Rt.

PIT No. 1

1/4, NE 1/4, Sec. 31, T50N, R9E, N.M. P.M.

FED. ROAD REG. NO.	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEETS
9	COLO.	C31-0291-00	35	

REV. Deletion of Rd. Appr. & Sd. Dr., Lt. Sta. 42+00
R.O.W. Sta. 50+ to 53+, Rt. A.B.H. 3/12/59



$\Delta = 22^\circ 57' 30''$ Lt.
 $T_s = 782.2'$
 $\Delta c = 14^\circ 57' 30''$
 $Dc = 2^\circ 00'$
 $Tc = 376.1'$
 $Lc = 747.9'$
 $Rc = 2865.0'$
 $S = .026'$
 $M.D.S. = 60$ M.P.H.
 $H.S.D. = 1120'$

SW 1/4, SE 1/4 SEC. 30

SE 1/4, SE 1/4, Sec. 30, T50N, R9E, N.M. P.M.

B.M. No. 7 Elev. 7059.19
Nail in Large Tree

B.M. No. 8 Elev. 7084.36

B.M. No. 9 Elev. 7100.00

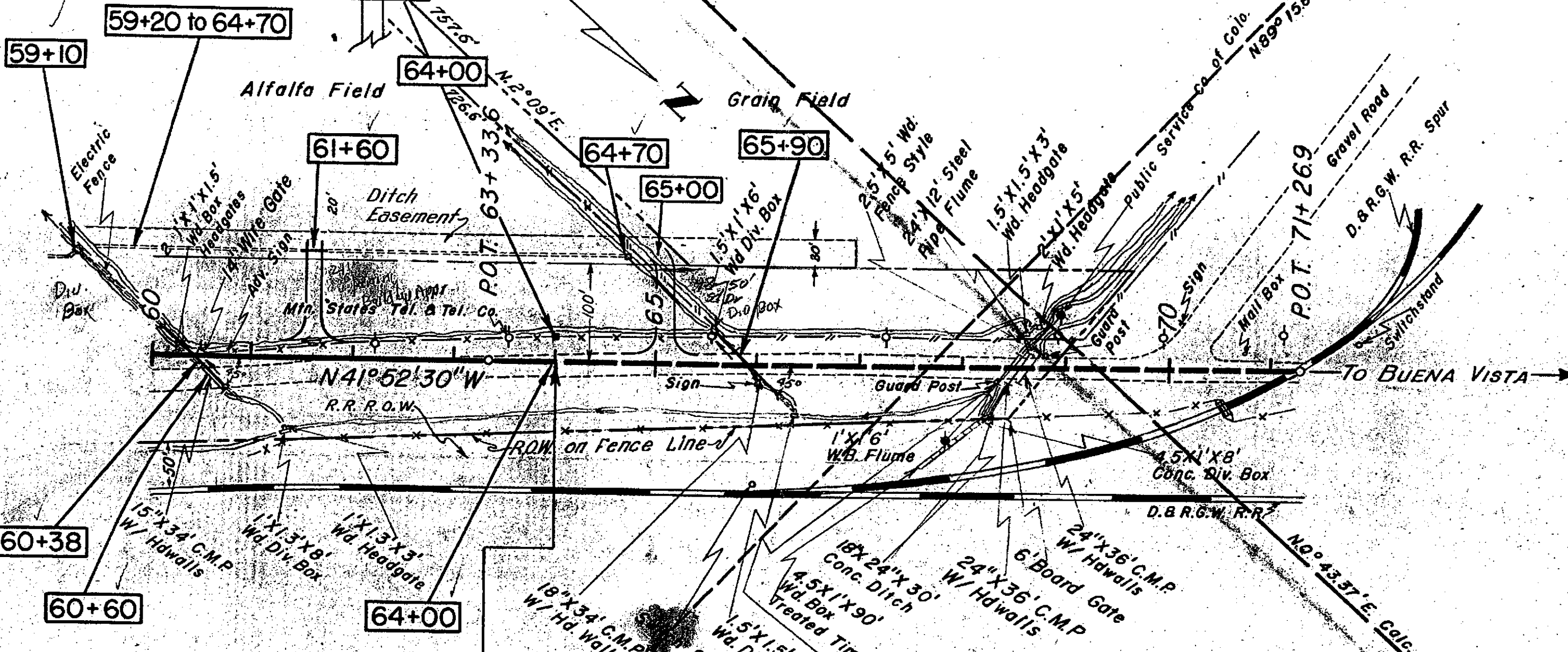
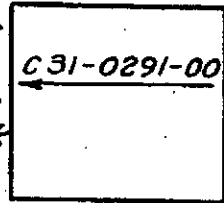
N.M. P.M.

issent Road

SW 1/4, SE 1/4, Sec. 30, T50N, R9E, N.M. P.M.

SE 1/4, SW 1/4, Sec. 30, T50N, R9E, N.M. P.M.

NE 1/4, SW 1/4



**STA. 64+00.0 END C 31-0291-00 =
STA. 1226+62.9 on FAP 248-A**

NOTE BOOK
ALIGNMENT CHECKED
RT. OF WAY CHECKED
NO. 21088

B.M. No. 10 Elev. 7121.55

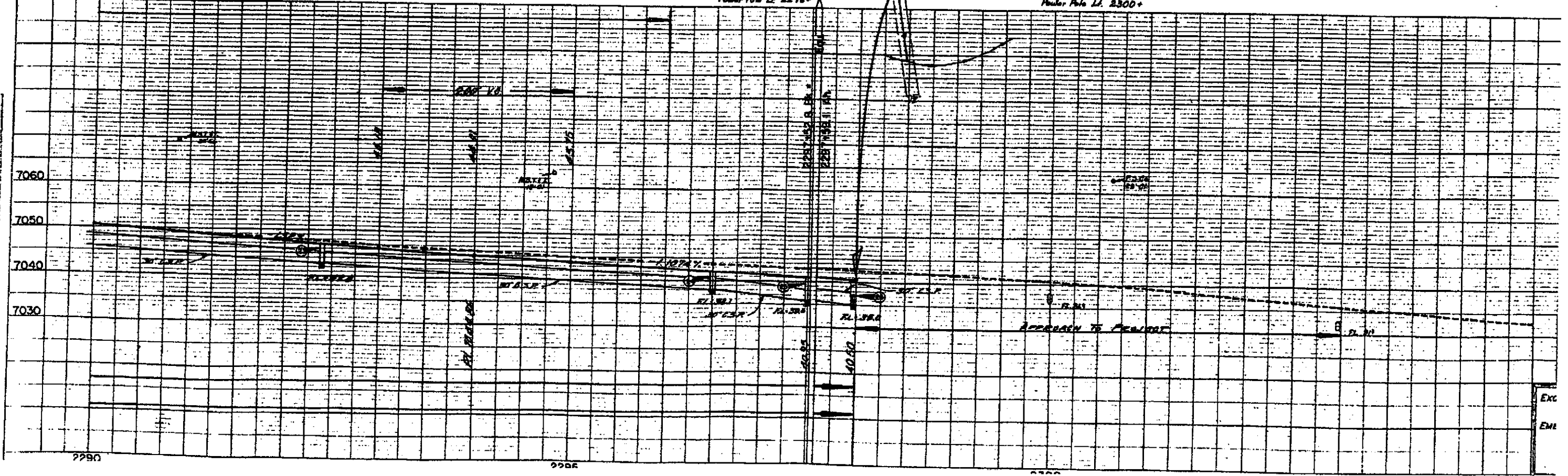
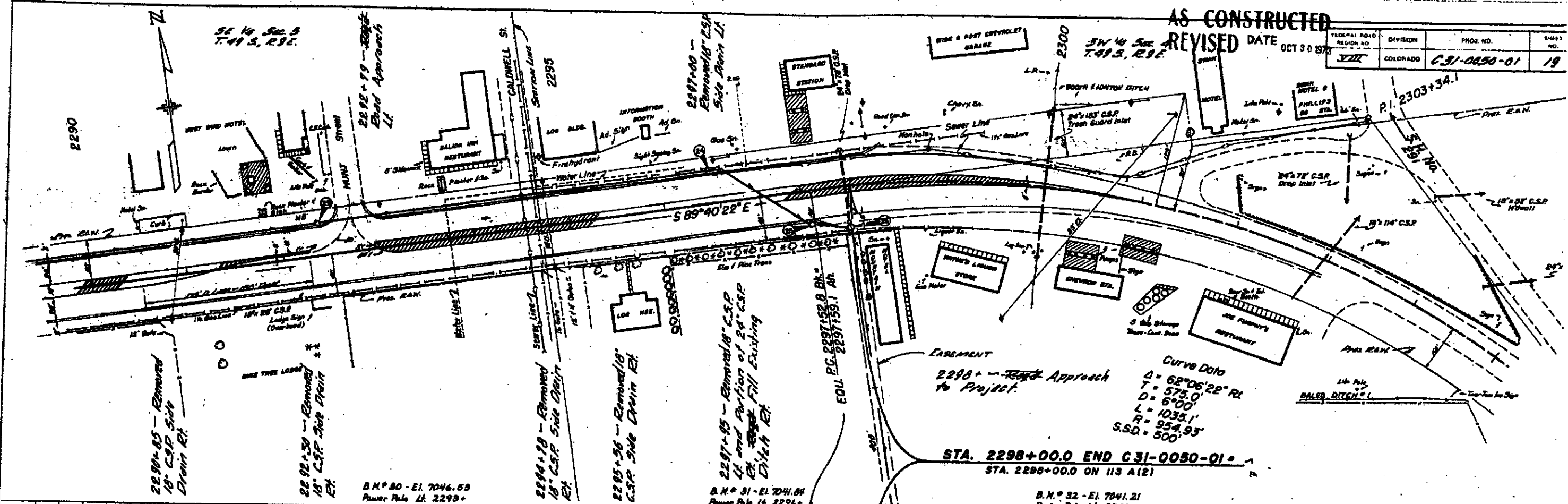
B.M. No. 10A Elev. 7127.24

NW 1/4, SE 1/4, Sec. 30, T50N, R9E, N.M. P.M.

B.M. No. 11 Elev. 7136.55

AS CONSTRUCTED
REVISED DATE OCT 30 1978

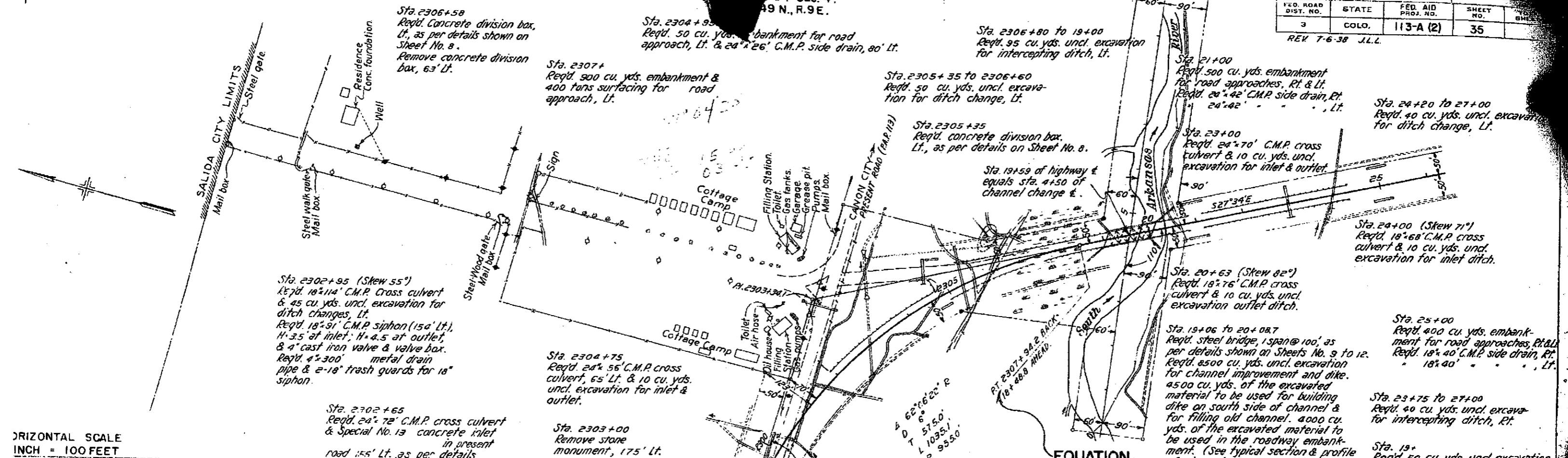
FEDERAL ROAD REGION NO.	DIVISION	PROJ. NO.	SHEET NO.
VIII	COLORADO	C-31-0050-01	19



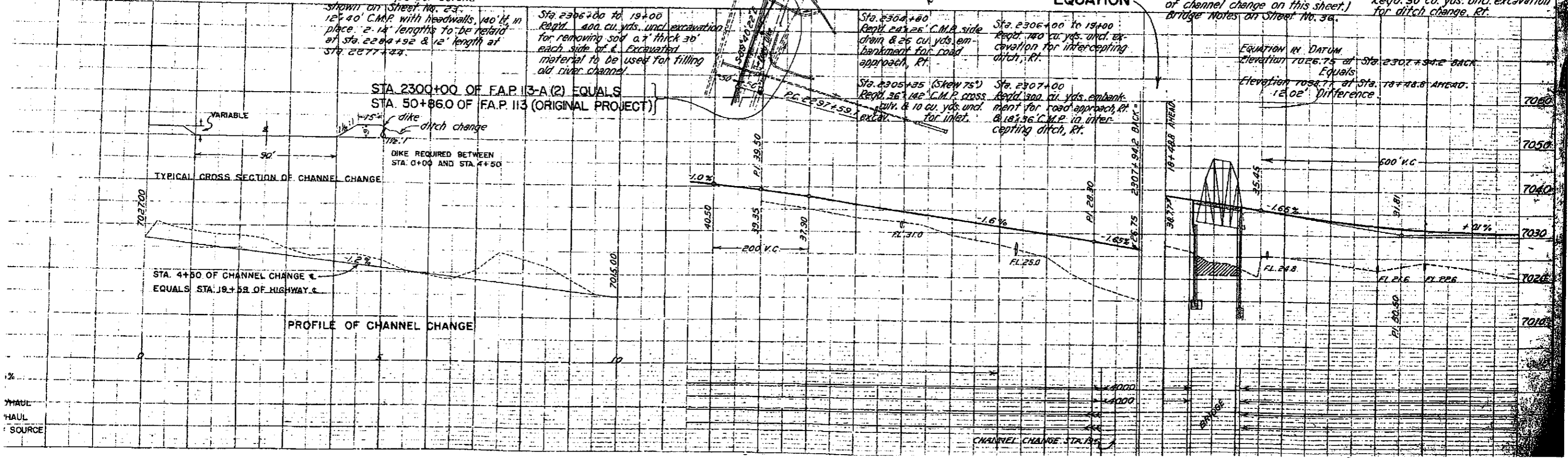
Exc
Emt

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
3	COLO.	113-A (2)	35	68

REV 7-6-38 J.L.L.



HORIZONTAL SCALE
1 INCH = 100 FEET



HAUL
HAUL
SOURCE

No. 22602 RT. OF WAY CHECKED

2298+70 ~ Req'd. 30' Driveway Rt.
 2299+80 ~ Req'd. 30' Driveway Rt.
~~2300+75 ~ Req'd. Reset Manhole, Lt.~~
~~2301+60 ~ Req'd. 30' Driveway Rt.~~
 2302+90 ~ Remove Side Drain Lt., Req'd. 24" x 100' Side Drain Lt. Relay 20'-24" C.S.P.
~~2303+20 ~ Req'd. 24" x 60' Side Drain Lt.~~
 2303+25 ~ Req'd. Road Approach Lt. (Detail on Sht. No. 14)
 2303+60 ~ Req'd. 30' Driveway Rt.

~~2298+00 ~ Req'd. 30' Driveway Lt.~~
 2299+70 ~ Req'd. 30' Driveway Lt.
 2302+95 ~ Plug Culvert.
 2303+75 ~ Remove Side Drain Lt.
 2304+00 ~ " " " " " "

2304+55 ~ Remove Side Drain Lt.
 18+50 24" x 30' Side Drain w/ Field Approach

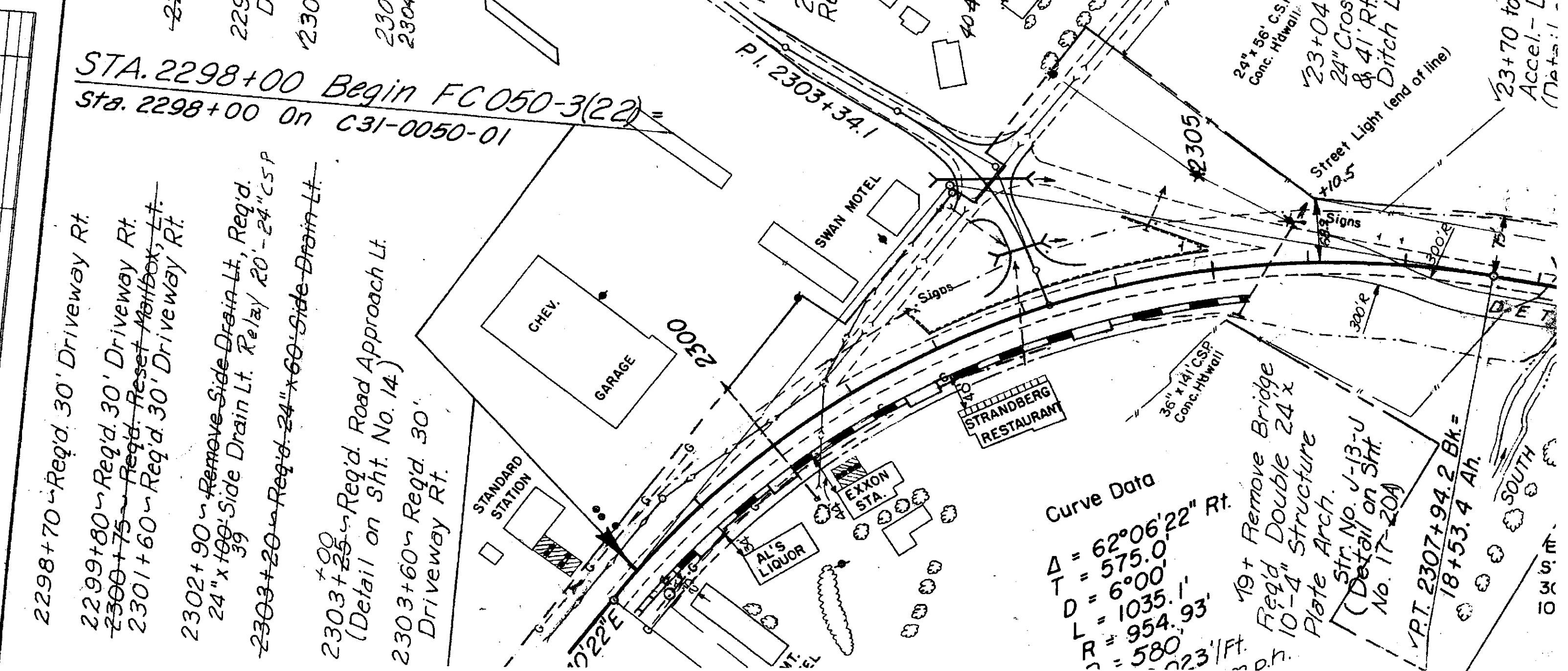
2306+ to 22+ ~ Req'd. Detour Rt.
 2306+ to 22+ ~ Remove Detour Rt.

20+65 ~ Req'd. Extend 18" Cross Culvert 32' Lt. & 40' Rt., End Sections Cover Exposed Culvert. (Detail on Sht. No. 13)

23+04 ~ Req'd. Extend 24" Cross Culvert 36' Lt. & 41' Rt., End Sections Ditch Lt. & Rt.
 23+70 to 39+25 ~ Req'd. Accel. - Decel. Lane Rt. (Notes: 1-2)

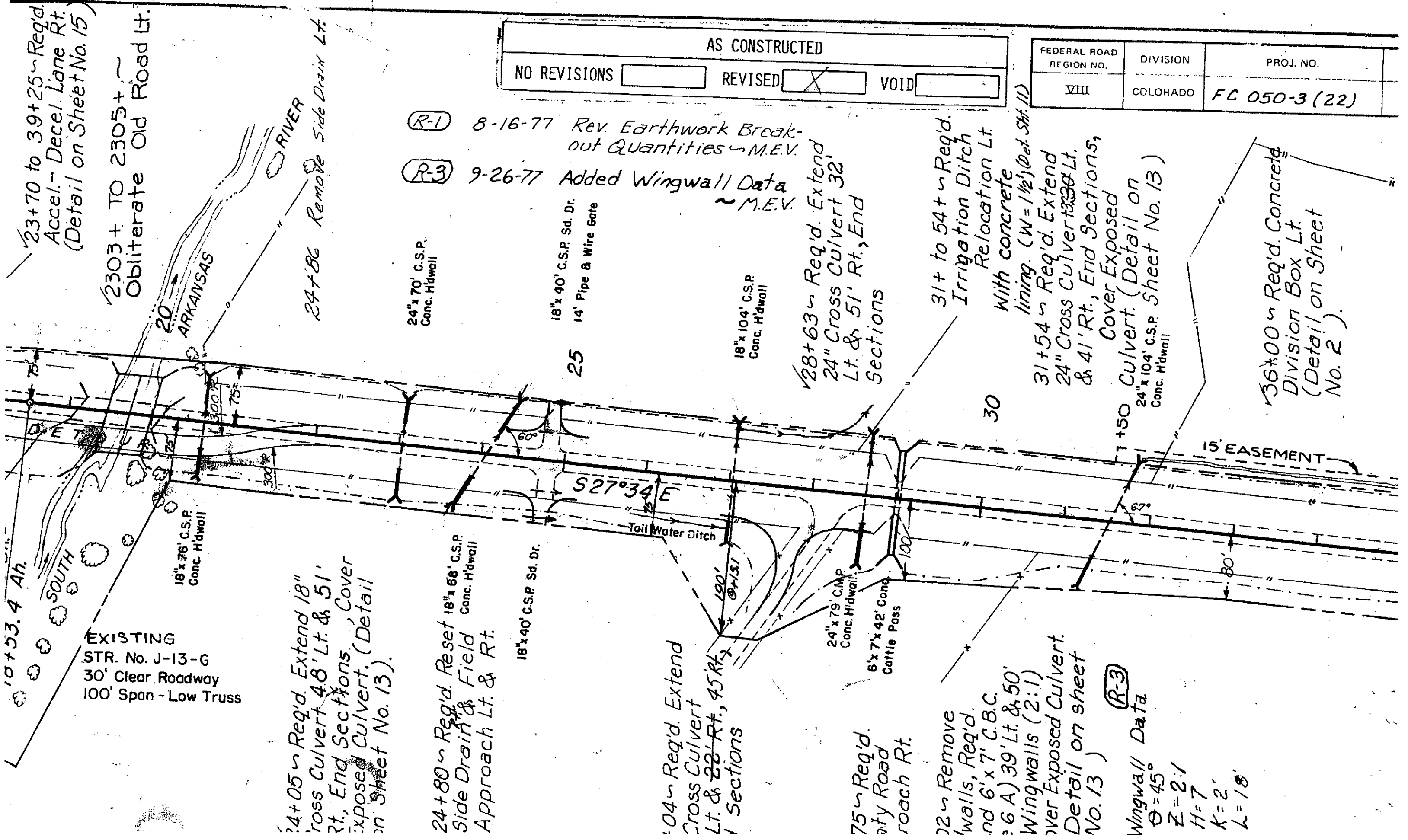
STA. 2298+00 Begin FC 050-3(22) = Sta. 2298+00 on C31-0050-01

P.I. 2303+34.1



Curve Data
 $\Delta = 62^{\circ}06'22''$ Rt.
 $D = 575.0$
 $L = 1035.1'$
 $R = 954.93'$
 $e = 23'$

19+ Remove Bridge Req'd. Double 24" x 10-4" Structure Plate Arch.
 (Sfr. No. J-13-U (Detail on Sht. No. 17-20A))
 V.P.T. 2307+94.2 Bk. = 18+53.4 Ah.



23+70 to 39+25 ~ Req'd. Accel. - Decel. Lane Rt. (Detail on Sheet No. 15)

2303+ TO 2305+ ~ Obliterate Old Road Lt.

24+05 ~ Req'd. Extend 18" Cross Culvert 48' Lt. & 51' Rt., End Sections, Cover Exposed Culvert. (Detail on Sheet No. 13).

24+80 ~ Req'd. Reset 18" x 68' C.S.P. Conc. Hdwall Side Drain & Field Approach Lt. & Rt.

25 ~ Req'd. Extend Cross Culvert Lt. & 22' Rt., 45' Rt., 1 Sections

25 ~ Req'd. City Road Approach Rt.

28 ~ Remove Walls, Req'd. and 6' x 7' C.B.C. Wingwalls (2:1) Cover Exposed Culvert. Detail on sheet No. 13)

Wingwall Data
 $\theta = 45^\circ$
 $Z = 2:1$
 $H = 7$
 $K = 2$
 $L = 18$

- (R-1) 8-16-77 Rev. Earthwork Break-out Quantities ~ M.E.V.
- (R-3) 9-26-77 Added Wingwall Data ~ M.E.V.

AS CONSTRUCTED		
NO REVISIONS	REVISED <input checked="" type="checkbox"/>	VOID

FEDERAL ROAD REGION NO.	DIVISION	PROJ. NO.
VIII	COLORADO	FC 050-3 (22)

24" x 70' C.S.P. Conc. Hdwall

18" x 40' C.S.P. Sd. Dr. 14' Pipe & Wire Gate

18" x 104' C.S.P. Conc. Hdwall

28+63 ~ Req'd. Extend 24" Cross Culvert 32' Lt. & 51' Rt., End Sections

31+ to 54+ ~ Req'd. Irrigation Ditch Relocation Lt. With concrete lining. (W = 1 1/2') (Det. Sht. 11)

31+54 ~ Req'd. Extend 24" Cross Culvert 33' Lt. & 41' Rt., End Sections, Cover Exposed Culvert. (Detail on Sheet No. 13)

24" x 104' C.S.P. Conc. Hdwall

36+00 ~ Req'd. Concrete Division Box Lt. (Detail on Sheet No. 2)

10+53.4 Ah. SOUTH

EXISTING STR. No. J-13-G
 30' Clear Roadway
 100' Span - Low Truss

18" x 76' C.S.P. Conc. Hdwall

18" x 40' C.S.P. Sd. Dr.

24" x 79' C.M.P. Conc. Hdwall

6' x 7' x 42' Conc. Cattle Pass

15' EASEMENT

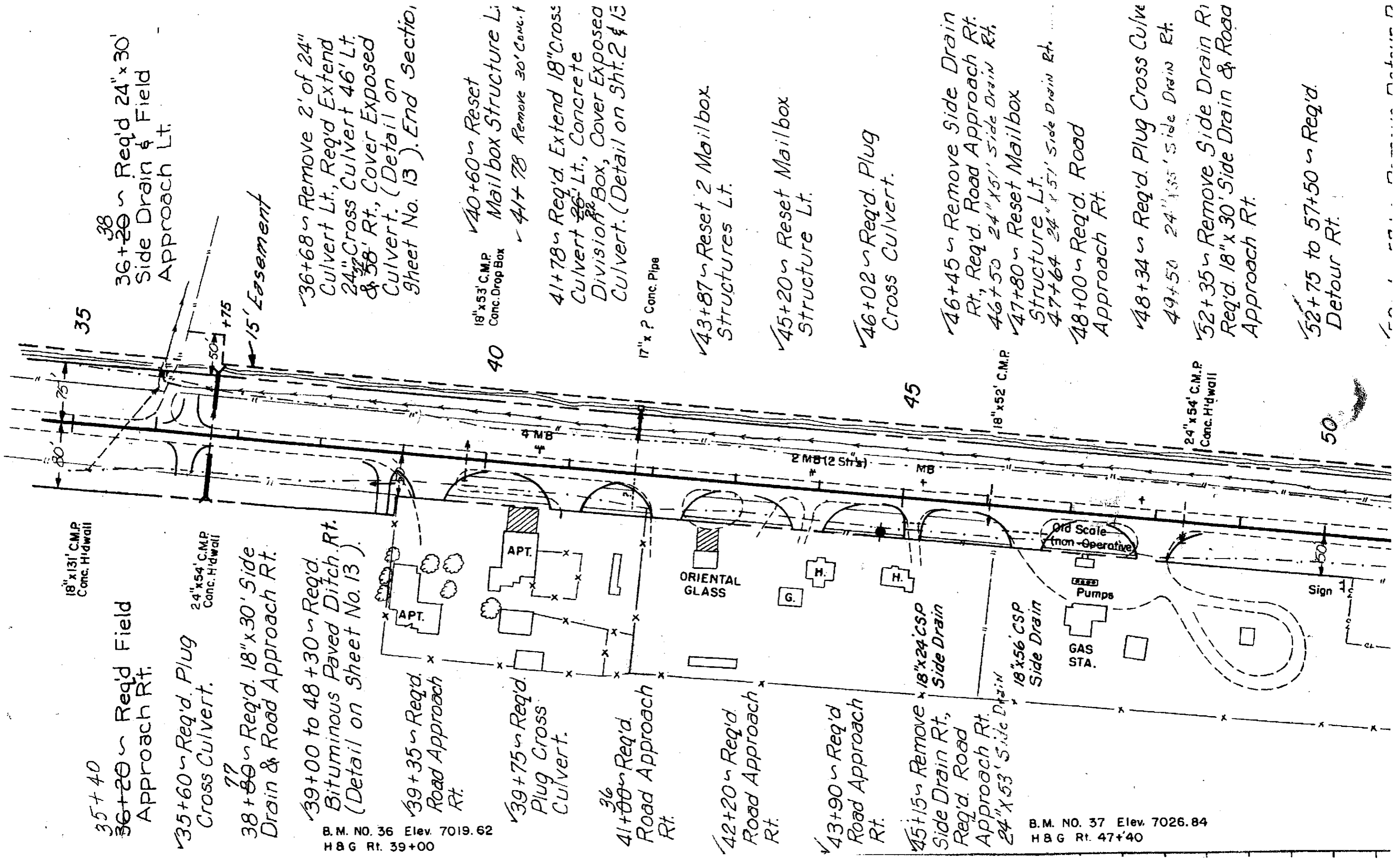
S 27° 34' E

Tail Water Ditch

190' @ 4:5.1

100'

80'



35+40

36+20 ~ Req'd Field Approach Rt.

35+60 ~ Req'd. Plug Cross Culvert.

38+80 ~ Req'd. 18"x30' Side Drain & Road Approach Rt.

39+00 to 48+30 ~ Req'd. Bituminous Paved Ditch. Rt. (Detail on Sheet No. 13).

39+35 ~ Req'd. Road Approach Rt.

39+75 ~ Req'd. Plug Cross Culvert.

41+00 ~ Req'd. Road Approach Rt.

42+20 ~ Req'd. Road Approach Rt.

43+90 ~ Req'd. Road Approach Rt.

45+15 ~ Remove 18"x24' CSP Side Drain Rt., Req'd. Road Approach Rt.

18"x56' CSP Side Drain

B.M. NO. 37 Elev. 7026.84
H&G Rt. 47+40

35

36+20 ~ Req'd 24"x30' Side Drain & Field Approach Lt.

15' Easement

36+68 ~ Remove 2' of 24" Culvert Lt., Req'd Extend 24" Cross Culvert 46' Lt. & 58' Rt., Cover Exposed Culvert. (Detail on Sheet No. 13). End Section.

40
18"x53' C.M.P. Conc. Drop Box

40+60 ~ Reset Mailbox Structure Lt.

41+78 ~ Req'd. Extend 18" Cross Culvert 26' Lt., Concrete Division Box, Cover Exposed Culvert. (Detail on Sht. 2 & 13)

17" x ? Conc. Pipe

43+87 ~ Reset 2 Mailbox Structures Lt.

45+20 ~ Reset Mailbox Structure Lt.

46+02 ~ Req'd. Plug Cross Culvert.

46+45 ~ Remove Side Drain Rt., Req'd. Road Approach Rt.

47+80 ~ Reset Mailbox Structure Lt.

48+00 ~ Req'd. Road Approach Rt.

48+34 ~ Req'd. Plug Cross Culvert

52+35 ~ Remove Side Drain Rt. Req'd. 18"x30' Side Drain & Road Approach Rt.

52+75 to 57+50 ~ Req'd. Detour Rt.

45

18"x52' C.M.P.

24"x54' C.M.P. Conc. Hd'wall

50

52+75 to 57+50 ~ Req'd. Detour Rt.

52+ to 57+ ~ Remove Detour.

54+40 ~ Req'd. Road Approach R.

55+25 ~ Remove Bridge. Req'd. 72" x 132' Cross Culvert with Type S Headwalls. (Drainage Data)

57+30 ~ Req'd. Road Approach Lt. & Rt.

54+10 18" x 20' Side Drain Lt.
54+55 18" x 20' "

53+88 ~ Relay and Extend 24" Cross Culvert 25' Lt. Division Box Lt. skew 71° Lt. (Detail on Sht. No. 13)

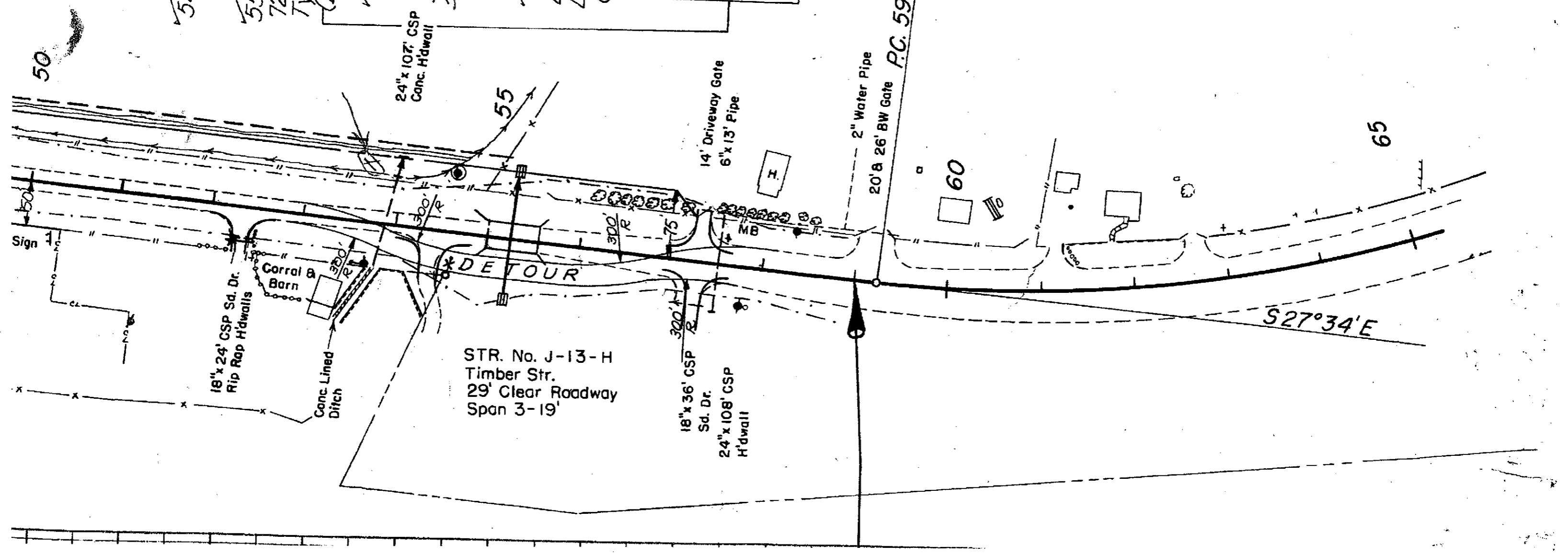
D.A. = 2.7 SQ. MI.
Q100 = 500 CFS
HW = 08.7
Q25 = 280 CFS
AHW = 7027.0
DHW = 7024.7

Rev. Earthwork Breakout Quant. M.E.V.

FEDERAL ROAD REGION NO.	DIVISION	PROJ. NO.	SHEET NO.	TOTAL SHEET
VIII	COLORADO	FC 050-3(22)	27	

AS CONSTRUCTED

NO REVISIONS REVISED VOID



92+50 ~ Req'd Road Approach Lt.

P.I. 90+95.4

96+63 ~ Req'd
Extend 24" Cros
Culvert 24' Lt.
22' Rt., Riprap
Rundown Lt.
End Sections
(RIPRAP SIZE = 12")

Conc. Foundation
Bldg. Burnt

6'x3'x2' Drop Inlet

8" x 40' B 8' x 60' Pipe
3'x3'x3' Drop Inlet

Chain Link Fence - 2 Propane Tan

S 88°12'E

Sign

CLEORA CEMETERY

24" x 72" CSP
Conc. H'dwall

Curve Data

$\Delta = 32^\circ 00'$ Rt.
 $T = 985.8'$
 $D = 1^\circ 40'$
 $L = 1920.0'$
 $R = 3438.0'$

$S = 0.045'/FT.$
H.S.D. > 600'
MDS = 60 M.P.H.

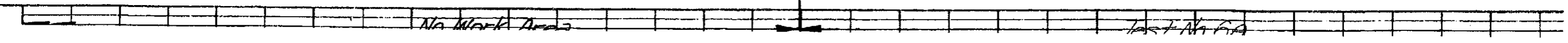
STA. 92+00 BEGIN FLH 050-3(21) =
Sta. 92+00 On C31-0050-03

B.M. NO. 42 Elev. 6987.02
H B G Rt. In Fence Line

P.T. 100+296.01

No Mark Area

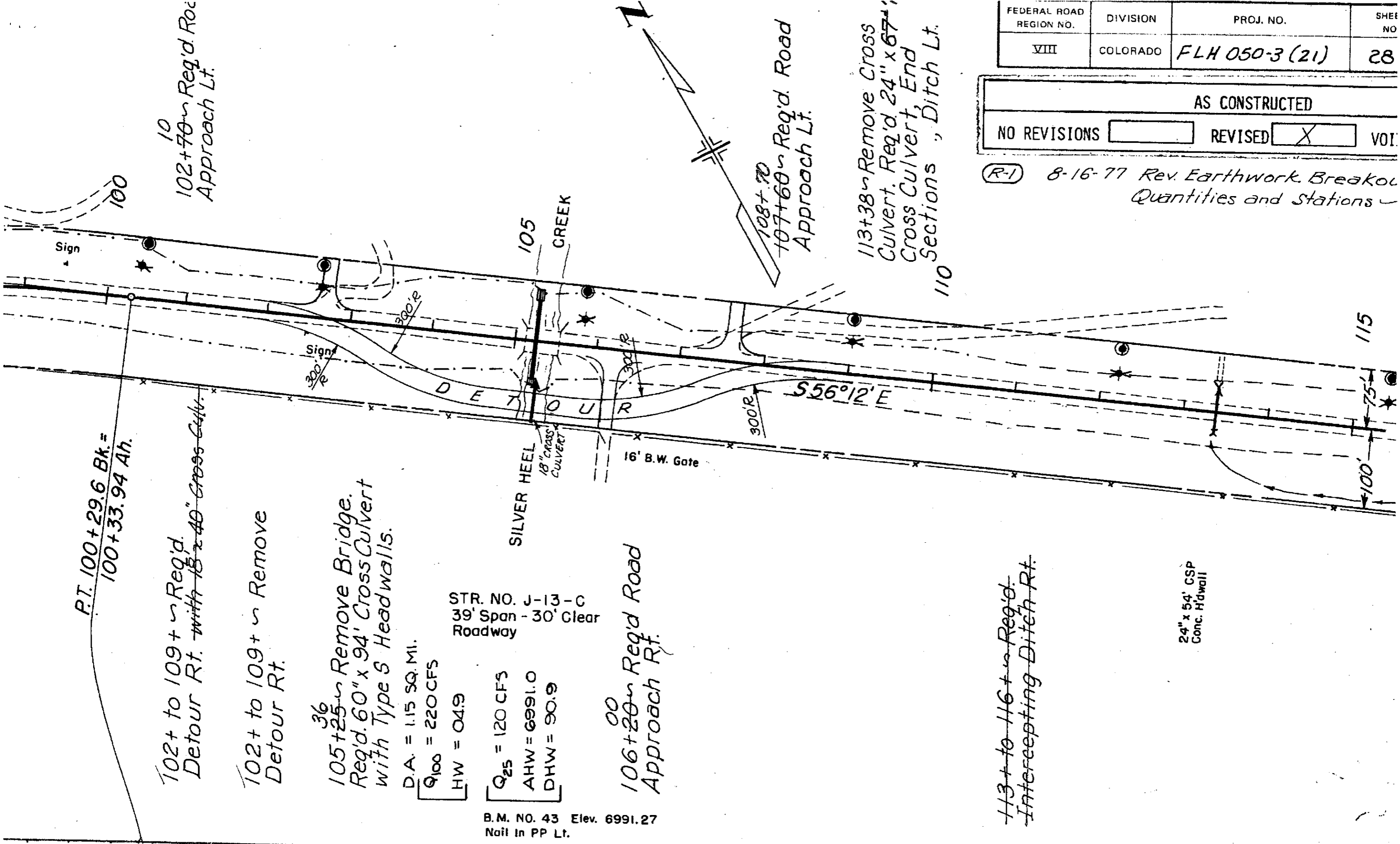
Last Mark



FEDERAL ROAD REGION NO.	DIVISION	PROJ. NO.	SHEET NO
VIII	COLORADO	FLH 050-3 (21)	28

AS CONSTRUCTED		
NO REVISIONS	<input type="checkbox"/>	REVISED <input checked="" type="checkbox"/> VOL.

(R-1) 8-16-77 Rev. Earthwork Breakout Quantities and Stations



P.T. 100+29.6 Bk. =
100+33.94 Ah.

102+ to 109+ ~ Req'd.
Detour Rt. with 15' x 40" Cross Culvert.

102+ to 109+ ~ Remove
Detour Rt.

105+25 ~ Remove Bridge.
Req'd. 60" x 94' Cross Culvert
with Type S Headwalls.

D.A. = 1.15 SQ. MI.
Q₁₀₀ = 220 CFS
HW = 04.9

Q₂₅ = 120 CFS
AHW = 6991.0
DHW = 90.9

STR. NO. J-13-C
39' Span - 30' Clear
Roadway

106+20 ~ Req'd Road
Approach Rt.

B.M. NO. 43 Elev. 6991.27
Nail in PP Lt.

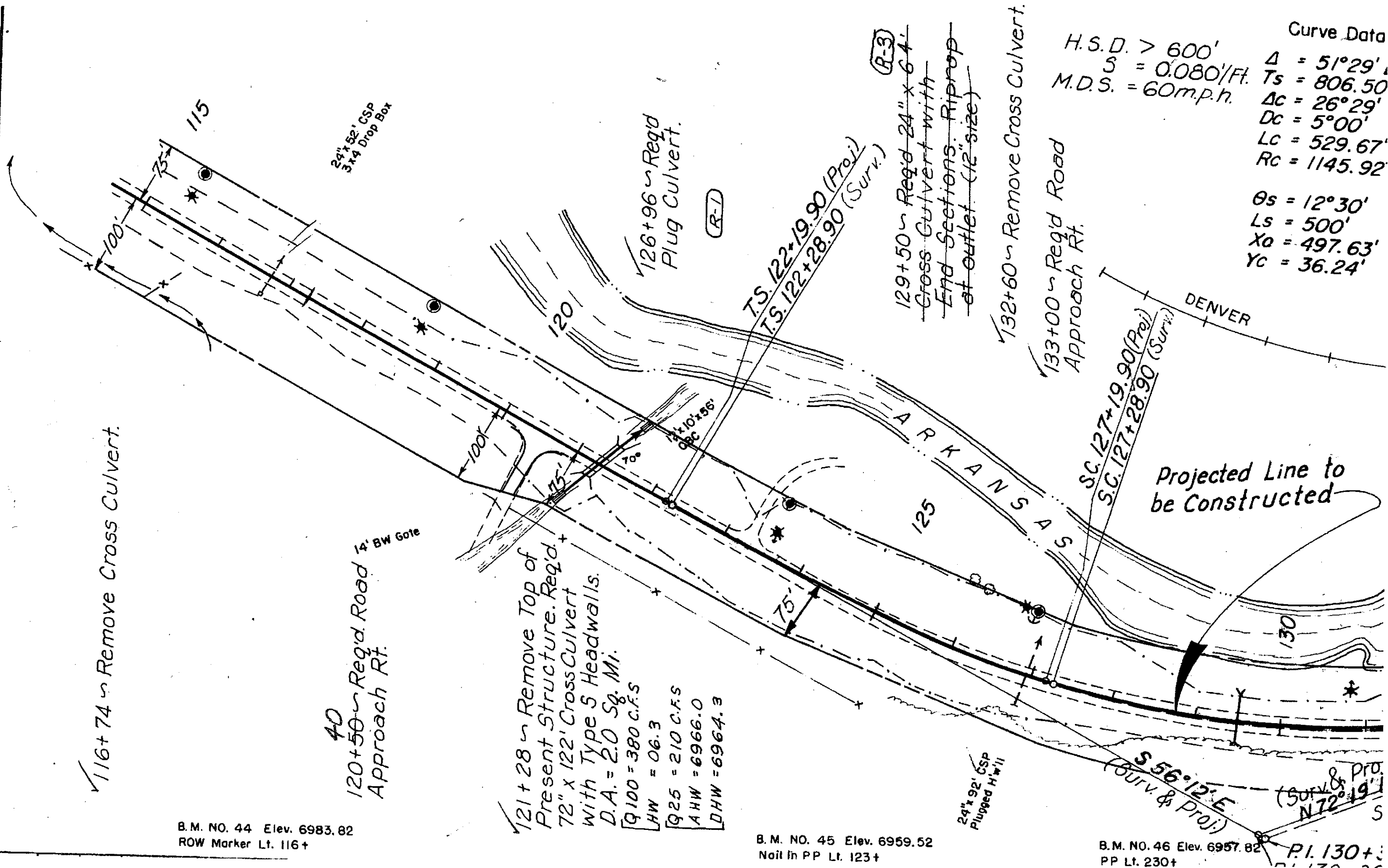
102+70 ~ Req'd. Road
Approach Lt.

108+70
107+60 ~ Req'd. Road
Approach Lt.

113+38 ~ Remove Cross
Culvert. Req'd. 24" x 67"
Cross Culvert, End
Sections, Ditch Lt.

113+ to 116+ ~ Req'd.
Intercepting Ditch Rt.

24" x 54' CSP
Conc. Hdwall



Curve Data

H.S.D. > 600'	$\Delta = 51^{\circ}29'$
$S = 0.080/\text{ft.}$	$T_s = 806.50$
M.D.S. = 60 m.p.h.	$\Delta c = 26^{\circ}29'$
	$Dc = 5^{\circ}00'$
	$Lc = 529.67'$
	$Rc = 1145.92'$
	$\theta_s = 12^{\circ}30'$
	$L_s = 500'$
	$X_0 = 497.63'$
	$Yc = 36.24'$

116+74 ~ Remove Cross Culvert.

40
120+50 ~ Req'd Road Approach Rt.
14' BW Gate

121+28 ~ Remove Top of Present Structure. Req'd 72" x 122' Cross Culvert with Type 5 Headwalls.
D.A. = 2.0 Sq. Mi.
Q100 = 380 c.f.s
HW = 06.3
Q25 = 210 c.f.s
AHW = 6966.0
DHW = 6964.3

B.M. NO. 45 Elev. 6959.52
Nail in PP Lt. 123+

B.M. NO. 46 Elev. 6957.82
PP Lt. 230+
P.I. 130+

Projected Line to be Constructed

(R-3)
129+50 ~ Req'd 24" x 6' Cross Culvert with End Sections. Riprap at outlet (12" size)

126+96 ~ Req'd Plug Culvert.
(R-1)

132+60 ~ Remove Cross Culvert.
133+00 ~ Req'd Road Approach Rt.

B.M. NO. 44 Elev. 6983.82
ROW Marker Lt. 116+

24" x 92' CSP Plugged H'w'll

S 56°12'E (Surv. & Proj.)
N 72°19'S (Surv. & Proj.)

29' Lt.
5.50'
29'
10'
1.67'
7.92'
10'
63'
4'

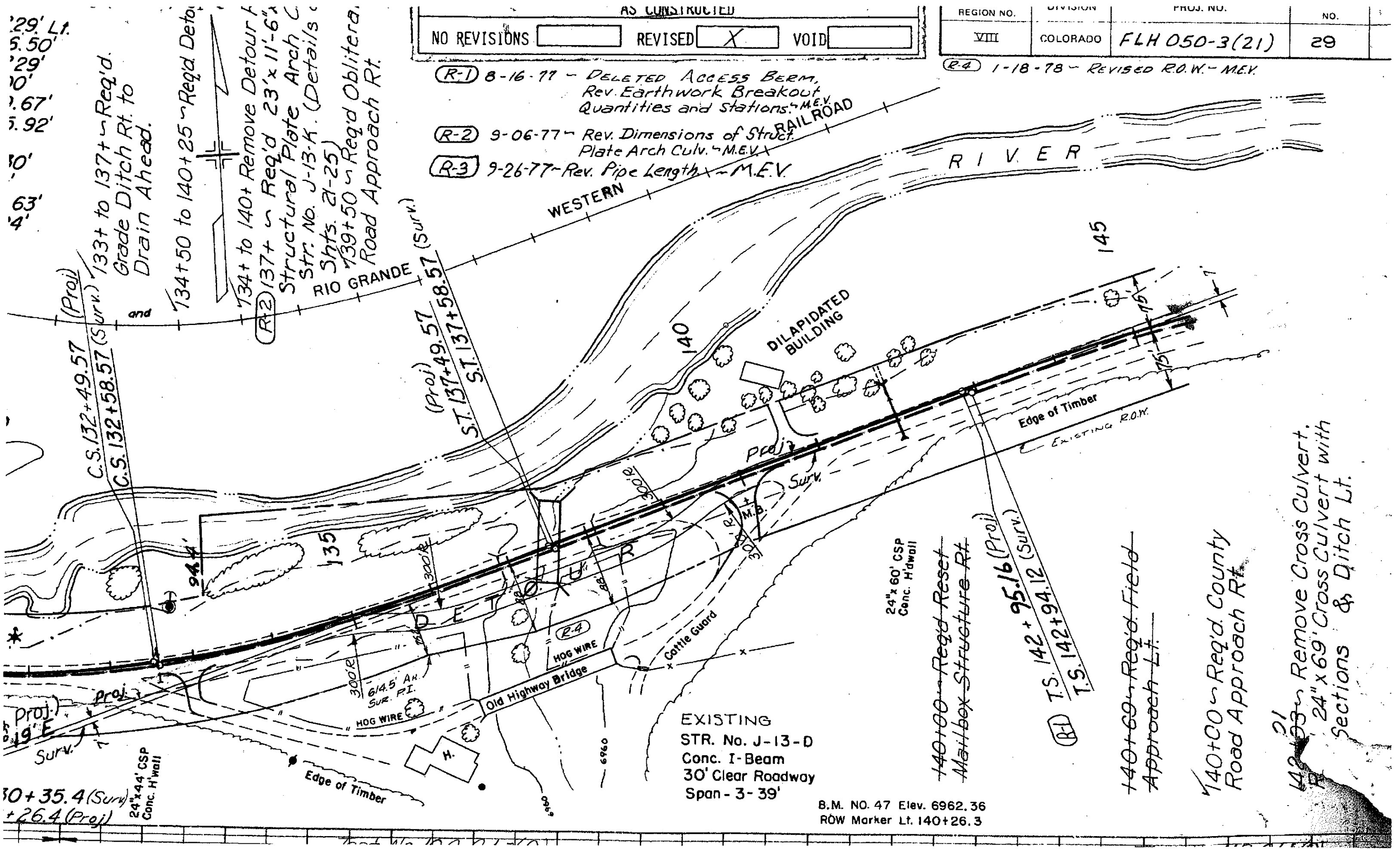
AS CONSTRUCTED

NO REVISIONS	REVISED	VOID
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REGION NO.	DIVISION	PROJ. NO.	NO.
VIII	COLORADO	FLH 050-3(21)	29

- (R-1) 8-16-77 - DELETED ACCESS BERM, Rev. Earthwork Breakout Quantities and Stations. - M.E.V.
- (R-2) 9-06-77 - Rev. Dimensions of Structural Plate Arch Culvert. - M.E.V.
- (R-3) 9-26-77 - Rev. Pipe Length. - M.E.V.

(R-4) 1-18-78 - REVISED R.O.W. - M.E.V.



133+ to 137+ ~ Req'd. Grade Ditch Rt. to Drain Ahead.

134+50 to 140+25 ~ Req'd. Detour

(R-2) 134+ to 140+ Remove Detour & Structural Plate Arch Culvert. Str. No. J-13-K. (Details: Shts. 21-25) 139+50 ~ Req'd. Obliterate Road Approach Rt.

130+35.4 (Surv.) + 26.4 (Proj.)

24"x44' CSP Conc. H'Wall

EXISTING STR. No. J-13-D Conc. I-Beam 30' Clear Roadway Span - 3-39'

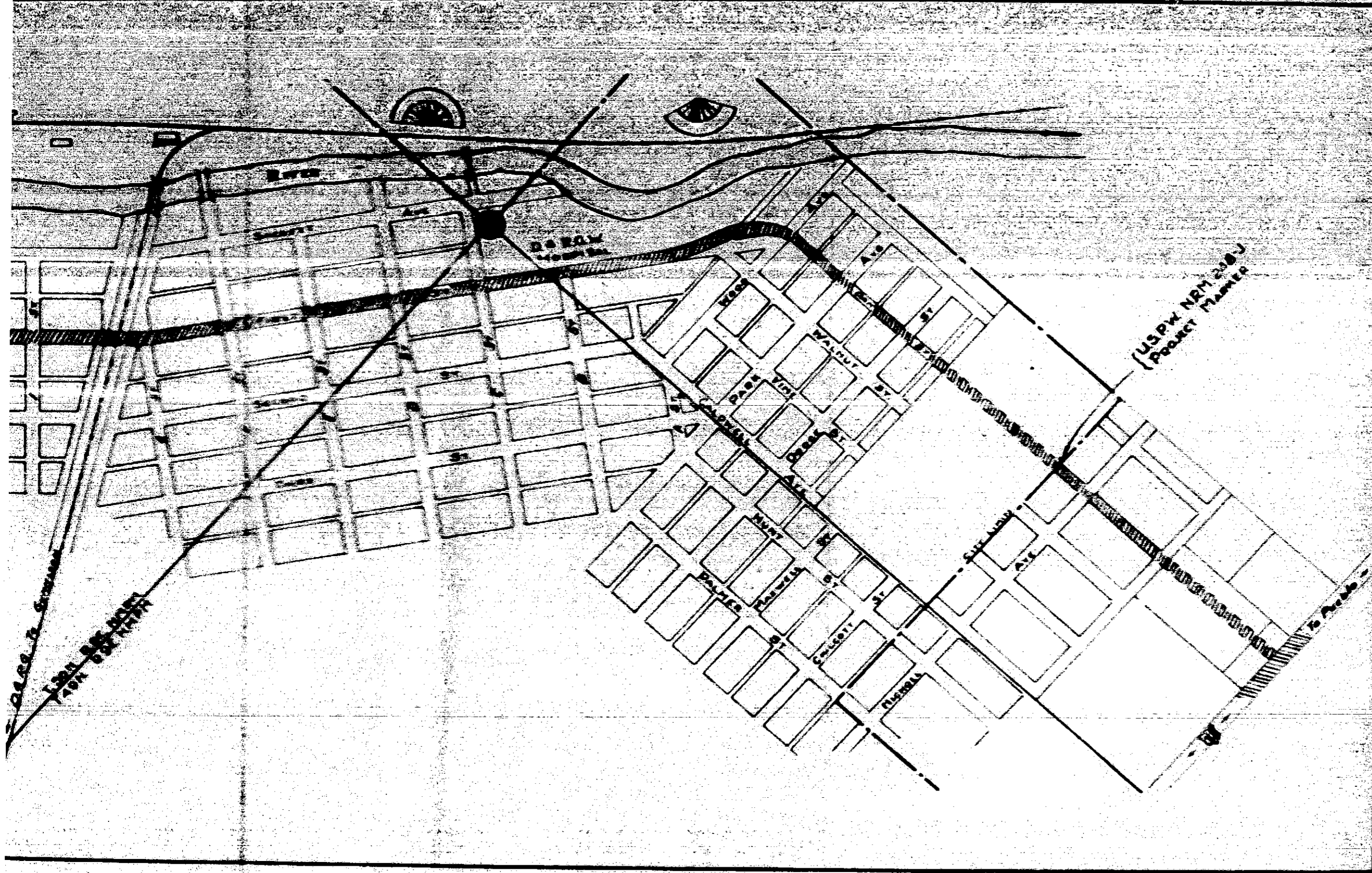
B.M. NO. 47 Elev. 6962.36
R.O.W. Marker Lt. 140+26.3

140+00 ~ Req'd. Reset Mailbox Structure Rt.

140+69 ~ Req'd. Field Approach Lt.

140+00 ~ Req'd. County Road Approach Rt.

142+93 ~ Remove Cross Culvert. 24" x 69" Cross Culvert with Sections & Ditch Lt.



(USPW NRM 230 U
Product Manager

COLORADO STATE HWY. DEPT.
MAP OF ROUTE OF U.S. 50
THROUGH SALIDA, COLO.

SCALE 1 IN. = 500 FT.
DRAWN BY J. H. [unclear] Sept. 10, 1934

